

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ.	FISCAL YEAR	INVEST. NO.	TOTAL SHEETS
				3	ARK.	9-257(5)			
				JOB NO.	424A, 425A, 426A, 427A, 428A		3	39	

SCHEDULE OF BRIDGE QUANTITIES - JOB 4613

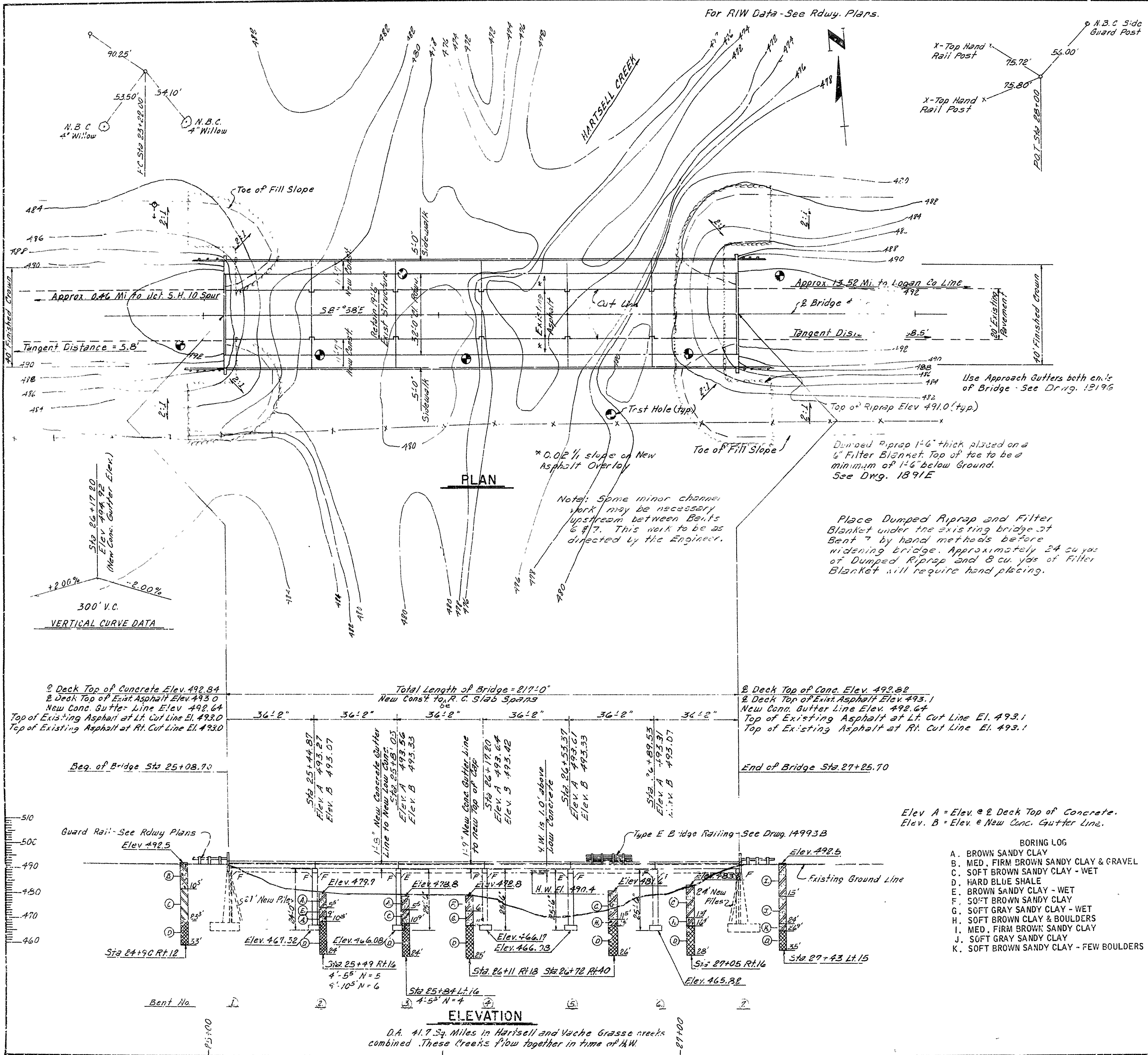
BRIDGE NO CODE NO		BRIDGE NAME PLATE TITLE	UNIT OF BRIDGE	ITEM NO	801	SP # 802	803	804	805	806	812	816	816	SP JOB 4613		
				ITEM	UNCLASSIFIED EXCAVATION FOR STRUCTURES- BRIDGE	CLASS S CONCRETE	BOILED LINSEED OIL	REINFORCING STEEL	STEEL BEARING PILING (HP 10 x 42)	(ALT 1) METAL (ALUMINUM) BRIDGE RAILING (TYPE S) (ALT 2) METAL (STEEL) BRIDGE RAILING (TYPE E)	BRIDGE NAME PLATES (TYPE C)	DUMPED RIPRAP	FILTER PLANKET	REMODELING EXISTING BRIDGES AND MAINTENANCE OF TRAFFIC		
				UNIT	CU YD	CU YD	GAL	LB	LIN. FT	LIN. FT	EACH	CU YD	CU YD	LUMP SUM		
424A X020	HARTSELL CREEK		BENT NO 1	26	7.01			715	84							
			BENT NO 2	55	14.33		1567									
			BENT NOS 3, 4, 5 & 6	203	59.05		6495									
			BENT NO 7	26	7.01		715	96								
			SPAN NOS 1 & 6		97.84	4.1	23,000		139.7	1						
			SPAN NOS 2, 3, 4 & 5		189.76	8.0	45,266		289.3							
TOTALS FOR BR NO 424A				310	375.00	12.1	77,780	150	429.0		286	96	0.44			
425A X020	WICHEGRASSE CREEK		BENT NOS 1 & 4	59	16.43			1832	160			294	96			
			BENT NOS 2 & 3	44	30.18		3083									
			SPAN NOS 1 & 3		98.36	4.1	23,370		138.9							
			SPAN NO 2		47.53	2.0	11,465		72.1							
			TOTALS FOR BR NO 425A				103	192.50	6.1	39,750	160	211.0	1	294	96	0.22
			426A X020	MOLLIES GAP		ABUTMENT NO 1		3.60		203						
ABUTMENT NO 2		3.32					200									
SPAN		54.88				1.8	11,317			1						
TOTALS FOR BR NO 426A							61.80	1.8	12,220			1			0.27	
ABUTMENT NO 1		4.90					324									
ABUTMENT NO 2		4.90					324									
427A X020	GIN CREEK		BENT NO 1	19	12.56			1360								
			SPAN NOS 1 & 2		79.32	3.5	20,032			1						
			TOTALS FOR BR NO 427A				19	101.70	3.5	22,090			1			0.14
			ABUTMENT NO 1	70	23.60		3103									
			ABUTMENT NO 2	64	22.51		3033									
			BENT NO	16	13.14		1452			1						
428A X020	PIGEON CREEK		SPAN NOS 1 & 2		72.35	2.9	17,142				1					
			TOTALS FOR BR NO 428A				150	131.50	2.9	24,730			1			0.13
			TOTALS FOR JOB 4613				* 582	862.50	26.4	176,570	340	640.0	5	** 582	94	1.00

\* 108 cu yds. estimated to be Rock Excavation  
\*\* 78 cu yds. will require hand placing.  
\*\*\* 26 cu yds. will require hand placing

SCHEDULE OF BRIDGE QUANTITIES  
GREENWOOD-LOGAN CO. LINE BRS. 8 APPRS.  
SEBASTIAN COUNTY  
ROUTE 10 SEC. 1  
ARKANSAS STATE HIGHWAY COMMISSION  
LITTLE ROCK, ARK.

DRAWN BY: H. H. H. DATE: 5-1-73  
TRACED BY: DATE: SCALE: 1" = 100'  
CHECKED BY: DATE: 5-8-73  
BRIDGE NO. 424A, 425A, 426A, 427A, 428A DRAWING NO. 18172

*Donald Robertson*  
BRIDGE ENGINEER



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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
				6	ARK.	5-252(3)			
				JOB NO.		4613		17	99
				SHEET NO.		424A		18	73

GENERAL NOTES

BENCH MARK - GREENWOOD 0.3 MI. EAST OF COURTHOUSE AT S.E. CORNER OF HWY. #40 BRIDGE OVER HARTSELL CREEK IN BASE OF CONCRETE RAILING CHISELED SQUARE 10" RT. STA. 27+25, ELEV. 493.49.

FOOTINGS SHALL BE SET A MINIMUM OF 1'-6" INTO MATERIAL DESIGNATED AS HARD BLUE SHALE. FOUNDATIONS FOR FOOTINGS SHALL BE PREPARED IN ACCORDANCE WITH ARTICLE 801.04 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL MAKE CHECK MEASUREMENTS OF THE EXISTING BRIDGE AND MAKE ADJUSTMENTS NECESSARY TO FIT THE NEW WORK TO THE EXISTING STRUCTURE.

ALL CONCRETE SHALL BE CURED IN THE DRY.

ALL PILING SHALL BE HP10X42 AND SHALL BE DRIVEN WITH AN APPROVED AIR, STEAM, OR DIESEL HAMMER WITH A MINIMUM ENERGY OF 10,000 FOOT LBS. PER BLOW TO A MINIMUM BEARING CAPACITY OF 55 TONS PER PILE. LENGTHS OF PILE SHOWN ARE FOR ESTIMATING QUANTITIES ONLY. ORDER LENGTHS SHOWN; CUT - OFF, OR BUILD-UP, IF NECESSARY, TO BE PAID FOR IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. PILES IN END BENTS ARE TO BE DRIVEN AFTER EMBANKMENT TO SURGRADE IS IN PLACE.

FOR DETAILS OF END BENTS, SEE DWG. NO. 18174

FOR DETAILS OF INTERMEDIATE BENTS, SEE DWG. NO. 18175

FOR DETAILS OF R.C. SLABS, SEE DWG. NO. 18176 & 18177

FOR DETAILS OF EXISTING BRIDGE, SEE DWG. NOS. 559 & 560.

SPECIFICATIONS: ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 1972, AND APPLICABLE SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO 1969

LIVE LOADING: H20 (NEW CONSTRUCTION)

UNIT STRESSES: CLASS "S" CONCRETE (N=10) 1,200 PSI

CLASS 3 CONCRETE (N=6) 1,600 PSI

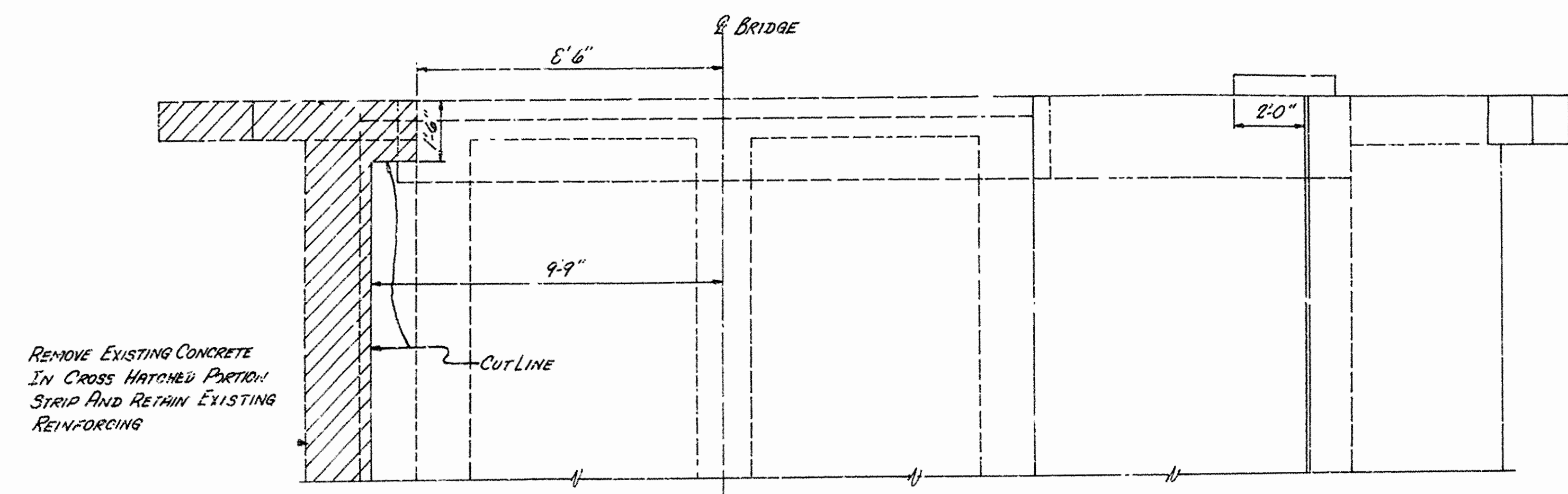
REINFORCING STEEL 20,000 PSI

Foundation Notes: See Drwg. 18174

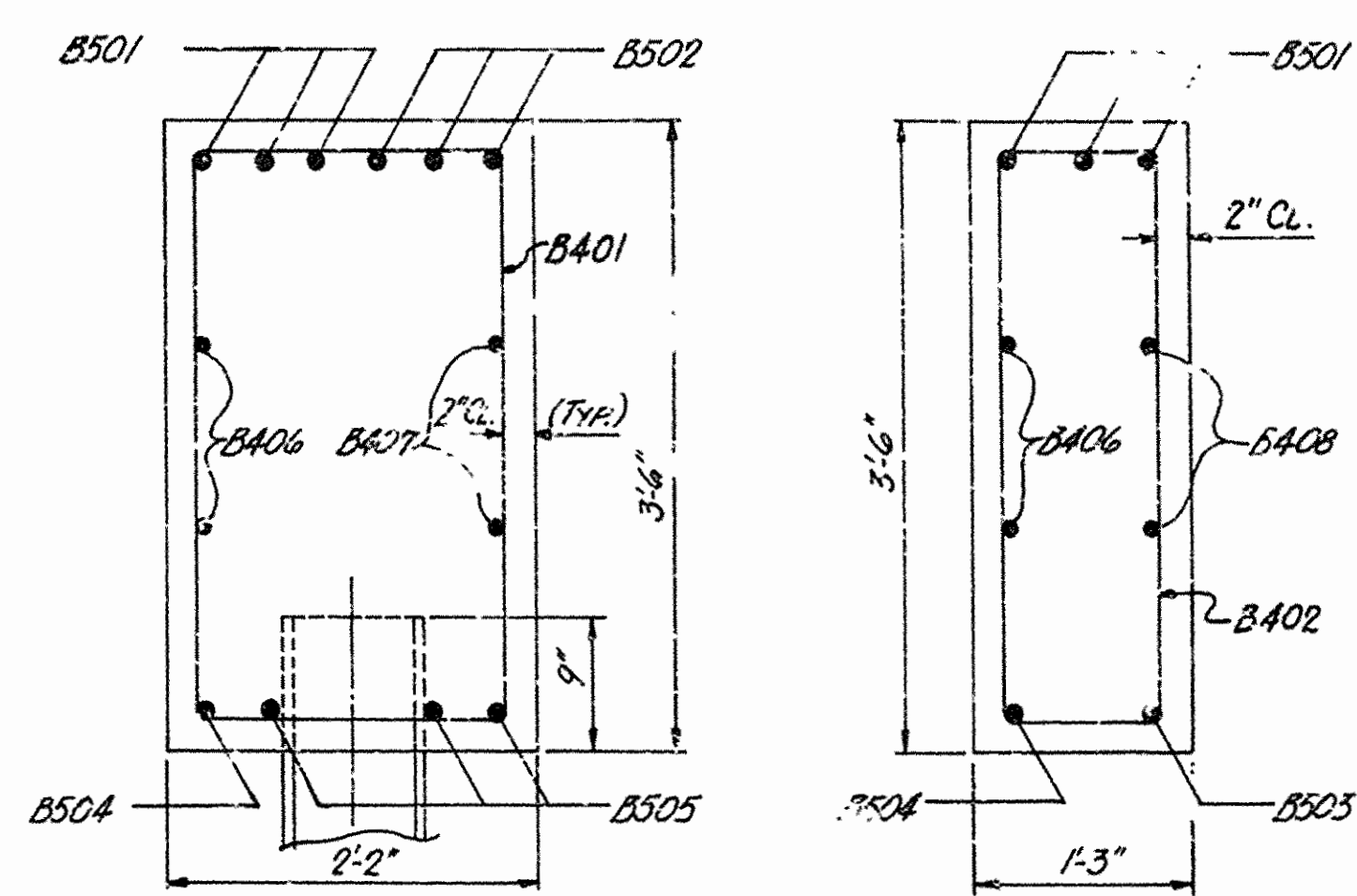


253

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
				6	ARK.	5-25-(8)		13	33
				JOB NO.		4413			
				S 4244 BENT DETAILS 18174					



PLAN OF WIDENING  
3/8" = 1'-0"



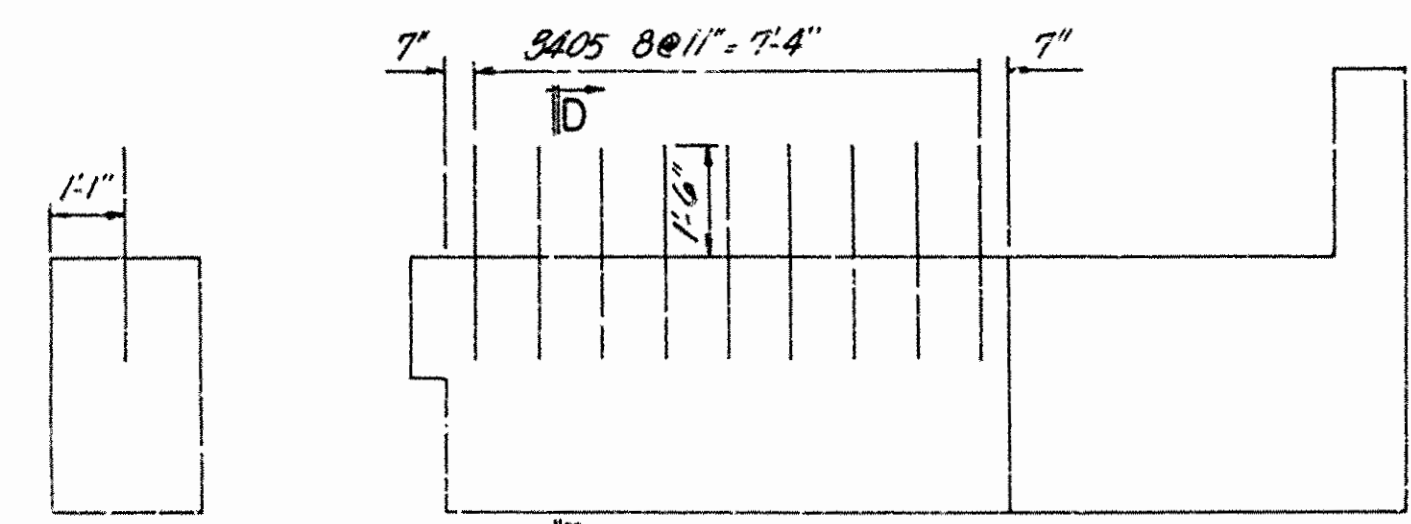
SECTION A-A  
3/8" = 1'-0"

SECTION B-B  
3/8" = 1'-0"

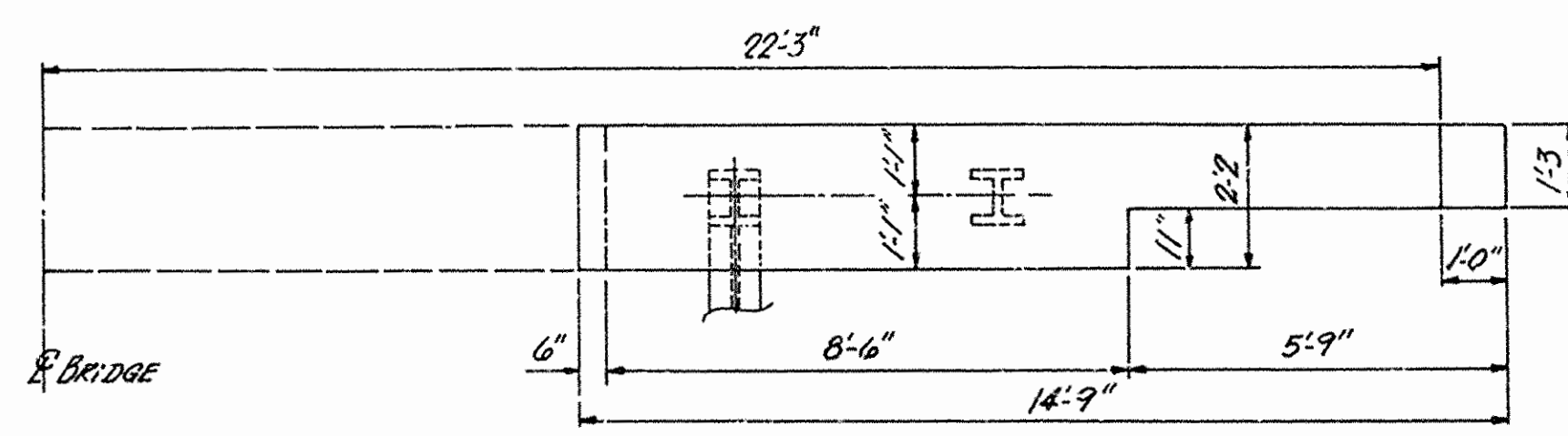
BAR LIST FOR ONE BENT (BOTH SIDES)

MARK	NO. REQD.	LENGTH	A	B	PIN DIA.	BENDING DIAGRAM
B601	8	5'-10"			STR.	
B501	6	15'-1"	13'-11"	5"	3 3/4"	
B502	6	9'-4"	8'-2"	5"	3 3/4"	
B503	2	7'-6"			STR.	
B504	2	13'-11"			STR.	
B505	6	8'-2"			STR.	
B401	20	10'-6"	1'-0"	3'-2"	2"	
B402	10	8'-8"	11"	3'-2"	2"	
B403	8	4'-0"	9"	1'-0"	2"	
B404	12	8'-0"	1'-0"	3'-2"	2"	
B405	18	3'-0"			STR.	
B406	4	13'-11"			STR.	
B407	4	8'-2"			STR.	
B408	4	7'-3"			STR.	

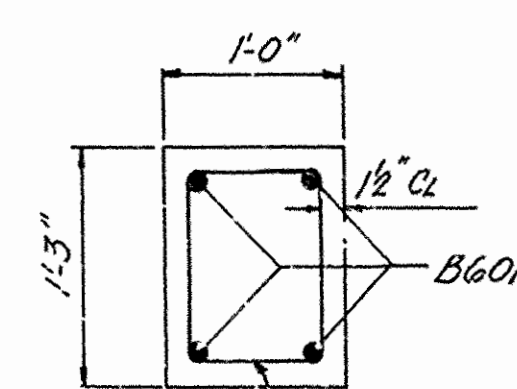
DIMENSIONS ARE OUT TO OUT OF BARS



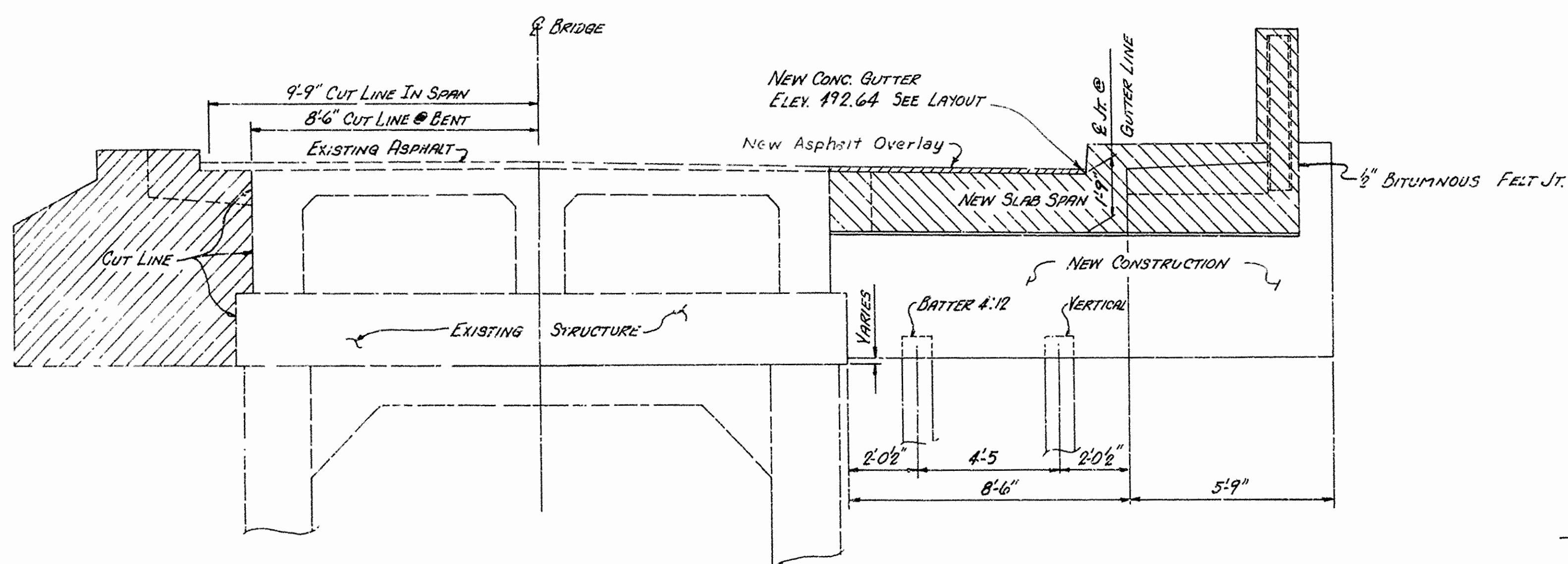
DETAILS OF SPAN DOWELS  
FIX. SPAN TO FIX. BENT  
N.T.S.



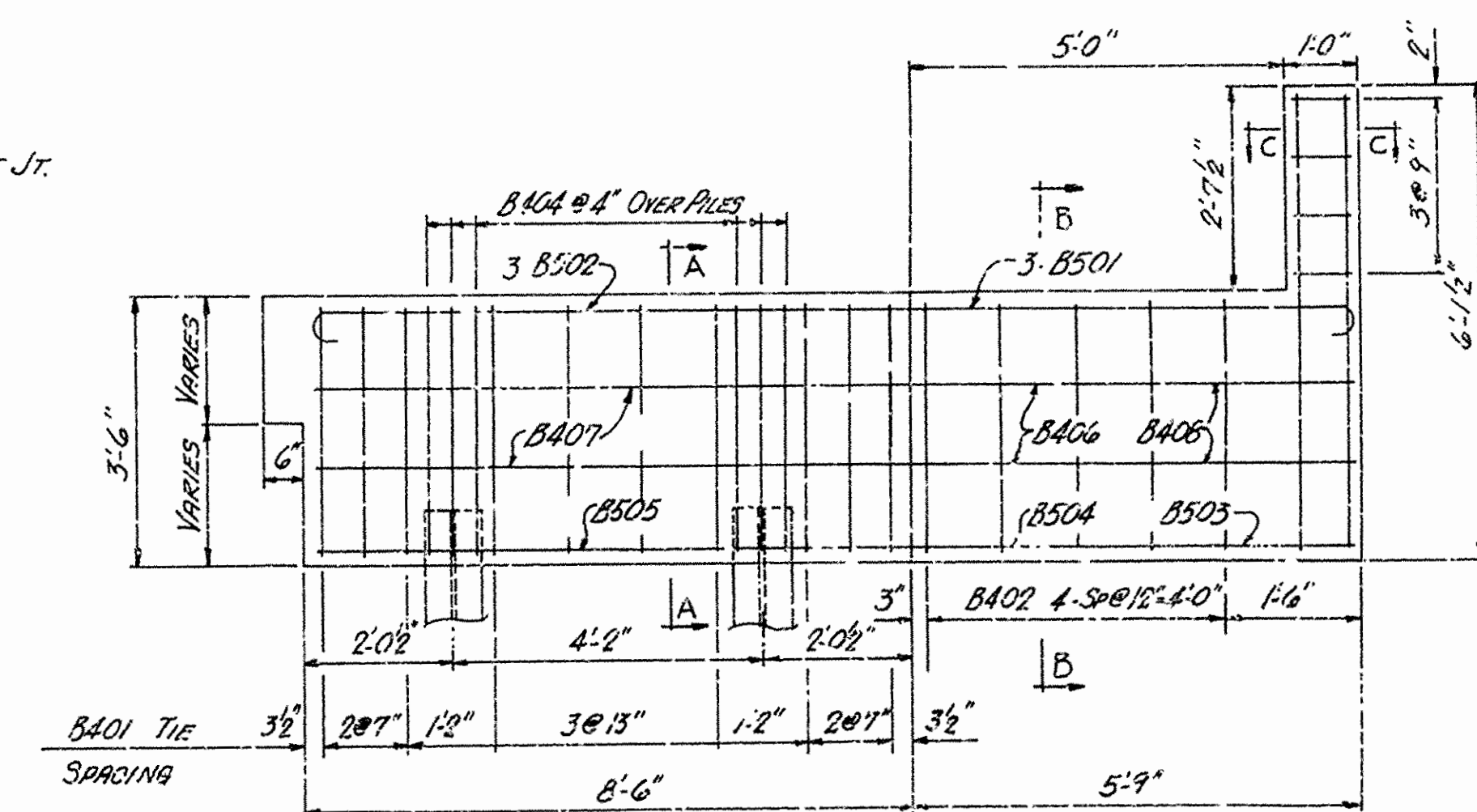
HALF PLAN OF BENT  
3/8" = 1'-0"



SECTION C-C  
1" = 1'-0"



SECTION THRU WIDENING  
3/8" = 1'-0"



HALF ELEVATION OF BENT  
1/2" = 1'-0"

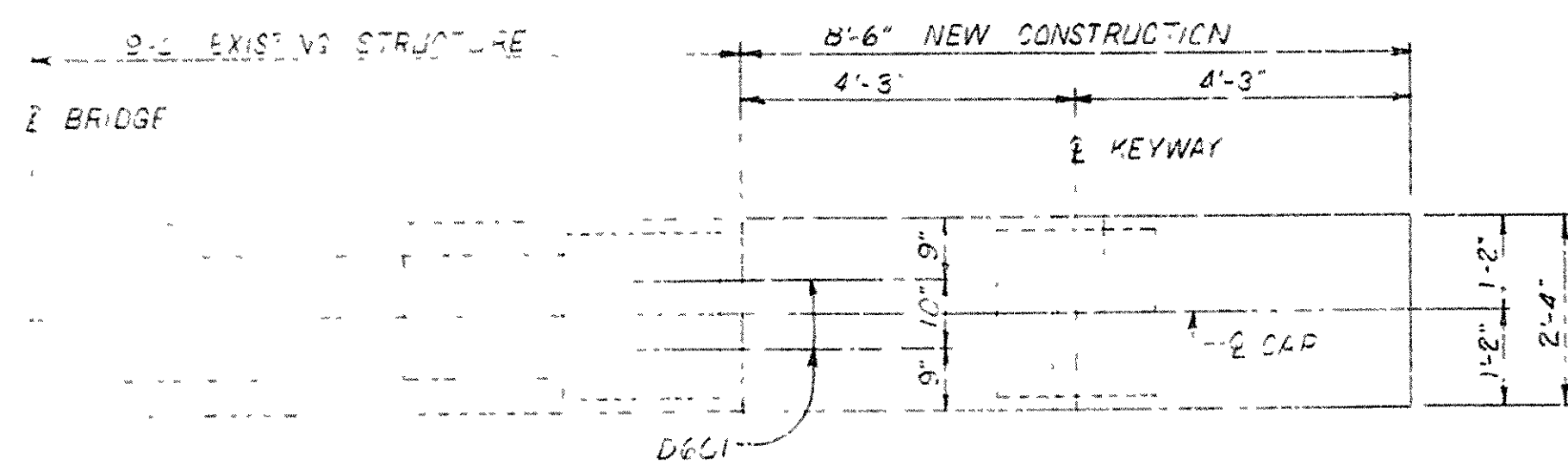
- NOTES:
- USE CLASS "S" CONCRETE (N=10)
  - BITUMINOUS FELT JOINT TO BE PAID FOR AS CLASS "S" CONCRETE.
  - ALL EXPOSED CORNERS TO HAVE 3/4" CHAMFER UNLESS OTHERWISE NOTED.
  - THE WORK CONTEMPLATED CONSISTS OF WIDENING THE EXISTING BRIDGE ON BOTH SIDES OF ROADWAY. FOR REQUIREMENTS IN CONDUCTING THE WORK, SEE JOB SPECIAL PROVISION "REMODELING EXISTING BRIDGES AND MAINTENANCE OF TRAFFIC". ALL DIMENSIONS RELATING TO EXISTING BRIDGE ARE TO BE VERIFIED IN THE FIELD AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE WIDENING TO EXISTING STRUCTURE.
  - REINFORCING STEEL TO BE ASTM A615, GRADE 40. REINFORCING STEEL IS TO BE HELD IN PLACE BY MEANS OF STEEL WIRE SUPPORTS SUFFICIENT IN NUMBER AND SIZE TO PROPERLY SUPPORT THE STEEL IN THE FORMS. THESE SHOP LISTS AND BENDING DIAGRAMS OF REINFORCING STEEL AND WIRE SUPPORTS MUST BE SUBMITTED AND APPROVAL SECURED BEFORE FABRICATION BEGUN.
  - FOR FILING SEE GENERAL NOTES DWG. NO. 18173

DETAILS OF WIDENING BENTS 1 & 7  
HARTSELL CREEK  
GREENWOOD-LOGAN CO. LINE BRS. & APPRS.  
SEBASTIAN COUNTY  
ROUTE 10 SEC. 1  
ARKANSAS STATE HIGHWAY COMMISSION  
LITTLE ROCK, ARK.

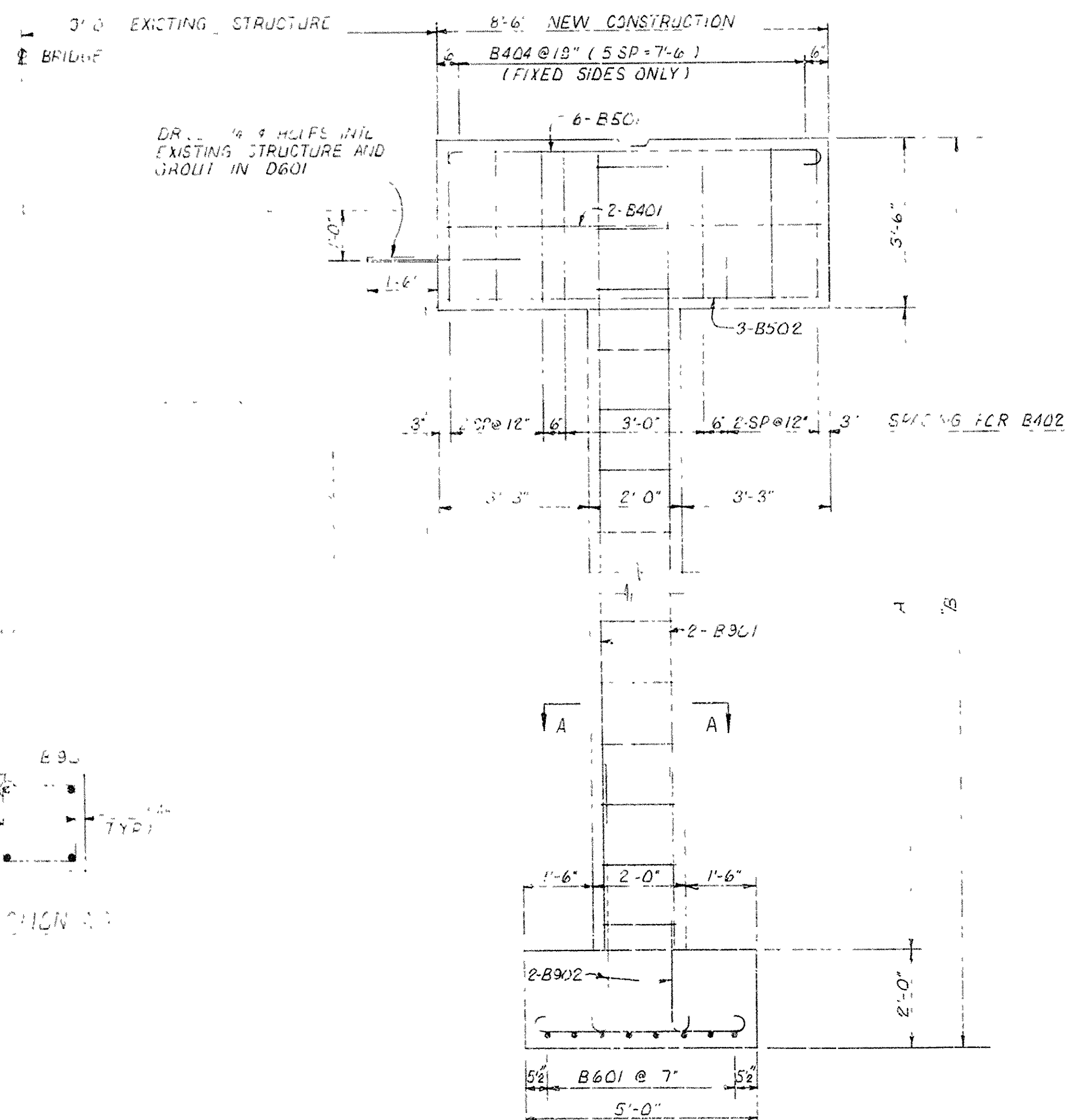
DRAWN BY: R.B. DATE: 1-18-73  
TRACED BY: DATE: 1-22-73  
CHECKED BY: DATE: 1-22-73  
SCALE: AS SHOWN  
BRIDGE NO. 424A DRAWING NO. 18174

BRIDGE ENGINEER

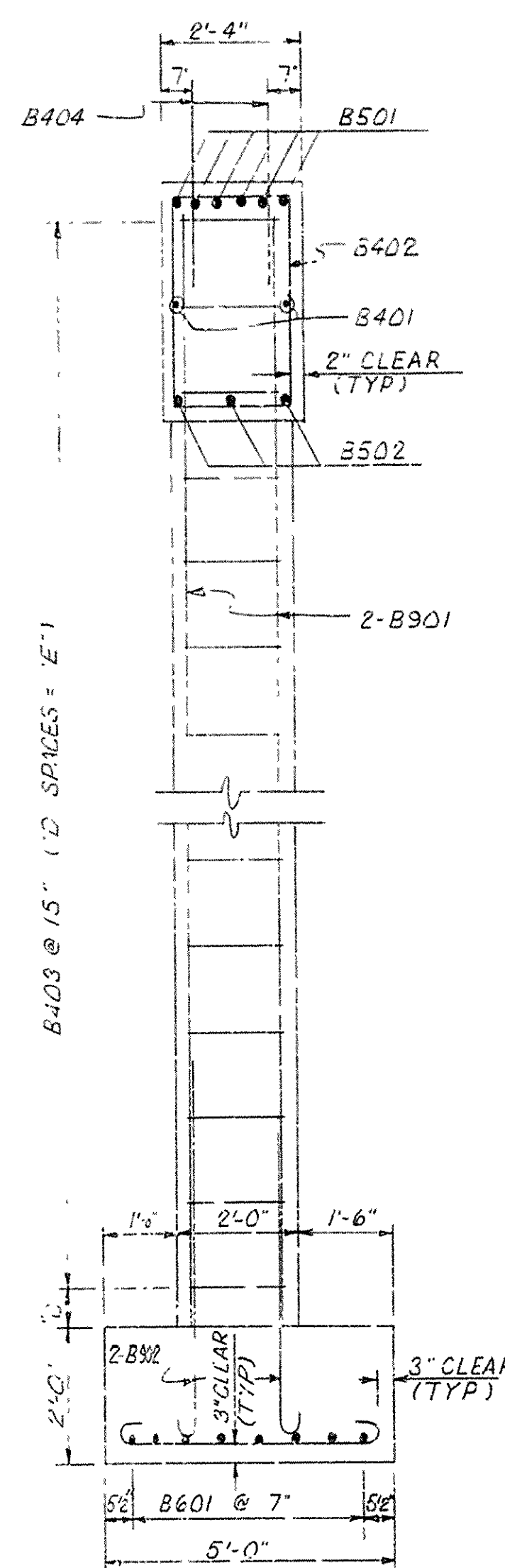
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STAT.	FED. AID FED.	FISCAL YR.	SHEET NO.	TOTAL SHEETS
				6	ARK.	5-15-63			
				JOB NO.		4613		19	99
						424A	BENT DETAILS	18175	



HALF PLAN



HALF ELEVATION



END VIEW

TABLE OF VARIABLES

VARIABLE	3	4	5	6
A	18'-0"	20'-0"	20'-0"	20'-0"
B	24'-0"	25'-6"	25'-6"	25'-6"
C	3"	6"	6"	6"
D	17	18	18	18
E	21'-3"	22'-6"	22'-6"	22'-6"
F	21'-6"	23'-0"	23'-0"	23'-0"
G	36	38	38	38
H	12	2	22	2

BAR LIST FOR ONE BENT (BOTH SIDES)

MARK	NO.	LENGTH	A	B	C	D	E	F	G	H	SENDING	RECEIVING
B301	5	—	—	—	—	—	—	—	—	—	—	—
B302	8	—	—	—	—	—	—	—	—	—	—	—
B303	32	5'-10"	4'-6"	0"	4'-6"	—	—	—	—	—	—	—
B304	12	3'-4"	3'-2"	0"	3'-4"	—	—	—	—	—	—	—
B305	0	8'-2"	—	—	—	—	—	—	—	—	—	—
B306	4	3'-2"	—	—	—	—	—	—	—	—	—	—
B307	6	2'-10"	2'-0"	3'-2"	2'-0"	—	—	—	—	—	—	—
B308	6	2'-10"	2'-0"	3'-2"	2'-0"	—	—	—	—	—	—	—
B309	4	3'-2"	—	—	—	—	—	—	—	—	—	—
B310	4	3'-2"	—	—	—	—	—	—	—	—	—	—

MEASUREMENTS ARE OUT TO OUT OF BARS

NOTES:

- USE CLASS "S" CONCRETE (W=10)
- BITUMINOUS FELT JOINT TO BE PAID FOR AS CLASS "S" CONCRETE.
- ALL EXPOSED CORNERS TO HAVE 3/4" CHAMFER UNLESS OTHERWISE NOTED.
- THE WORK CONTEMPLATED CONSISTS OF WIDENING THE EXISTING BRIDGE ON BOTH SIDES OF THE ROADWAY. FOR REQUIREMENTS IN CONDUCTING THE WORK, SEE JOB SPECIAL PROVISION "REMODELING EXISTING BRIDGES AND MAINTENANCE OF TRAFFIC". ALL DIMENSIONS RELATING TO EXISTING BRIDGE ARE TO BE VERIFIED IN THE FIELD AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE WIDENING TO EXISTING STRUCTURE.
- REINFORCING STEEL TO BE ASTM A615, GRADE 40. REINFORCING STEEL IS TO BE HELD IN PLACE BY MEANS OF STEEL WIRE SUPPORTS SUFFICIENT IN NUMBER AND SIZE TO PROPERLY SUPPORT THE STEEL DIAGRAM OF REINFORCING STEEL AND WIRE SUPPORTS MUST BE SUBMITTED AND APPROVAL SECURED BEFORE FABRICATION IS BEGUN.

DETAILS OF WIDENING BENTS 2-6  
HARTSELL CREEK  
GREENWOOD-LOGAN CO. LINE BRS. & APPRS  
SEBASTIAN COUNTY  
ROUTE 10 SEC. 1  
ARKANSAS STATE HIGHWAY COMMISSION  
LITTLE ROCK, ARK.

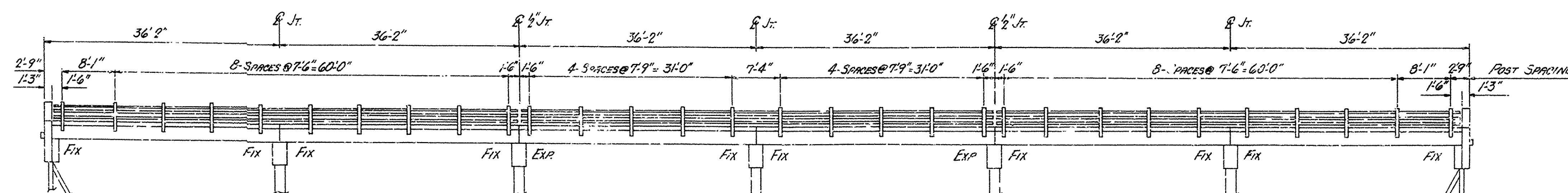
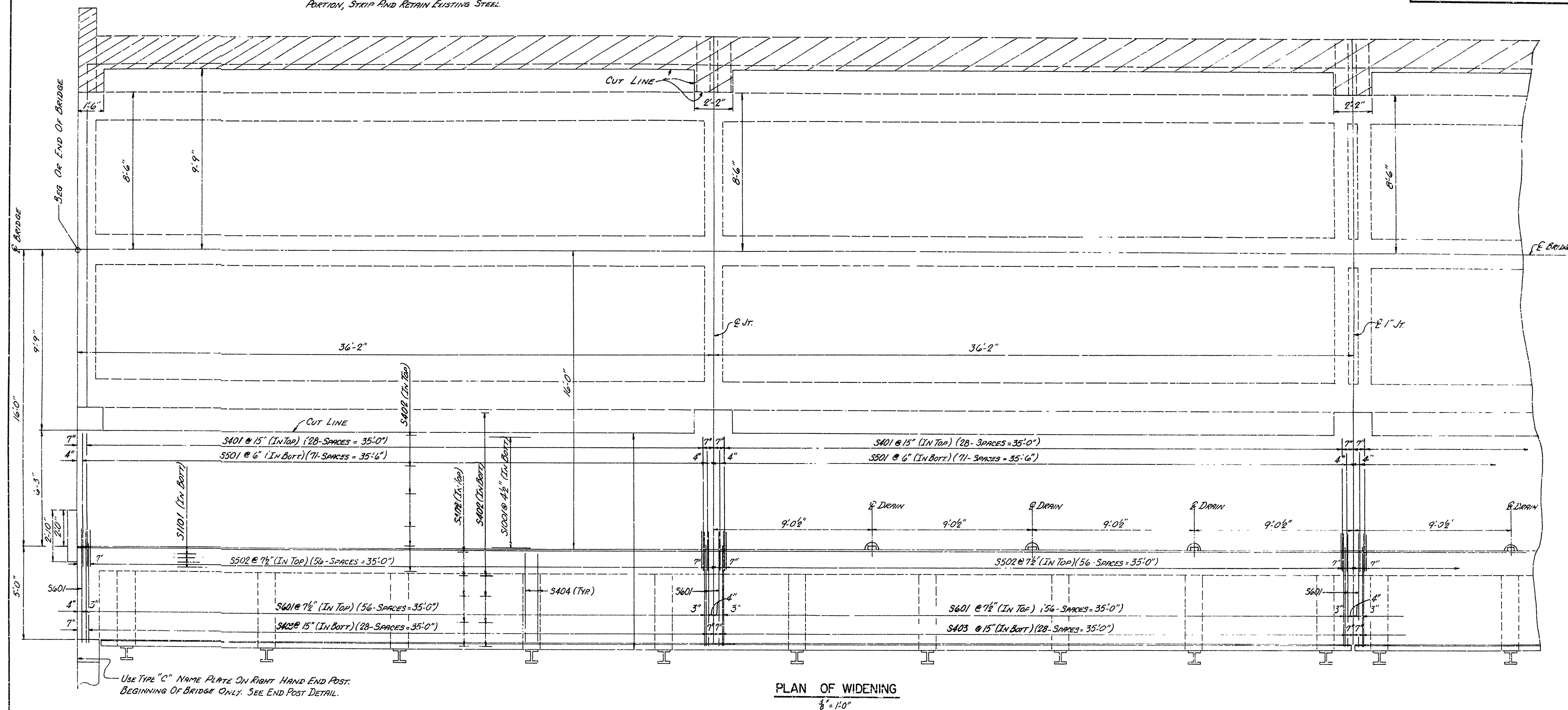
DRAWN BY: JDR DATE: 1-15-73  
CHECKED BY: EYD DATE: 1-22-73  
SCALE: 1/2" = 1'-0"  
BRIDGE NO. 424A DRAWING NO. 18175

Wesley Pinkerton  
BRIDGE ENGINEER



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S (1) 424A SPAN DETAILS 12/72



DETAIL SHOWING TYPE "E" RAIL POST SPACING  
1/2" = 1'-0"

SHEET 1 OF 2  
DETAIL OF WIDENING SPANS  
HARTSELL CREEK  
GREENWOOD-LOGAN CO. LINE BRS. & APPRS.  
SEBASTIAN COUNTY  
ROUTE 10 SEC. 1  
ARKANSAS STATE HIGHWAY COMMISSION  
LITTLE ROCK, ARK.

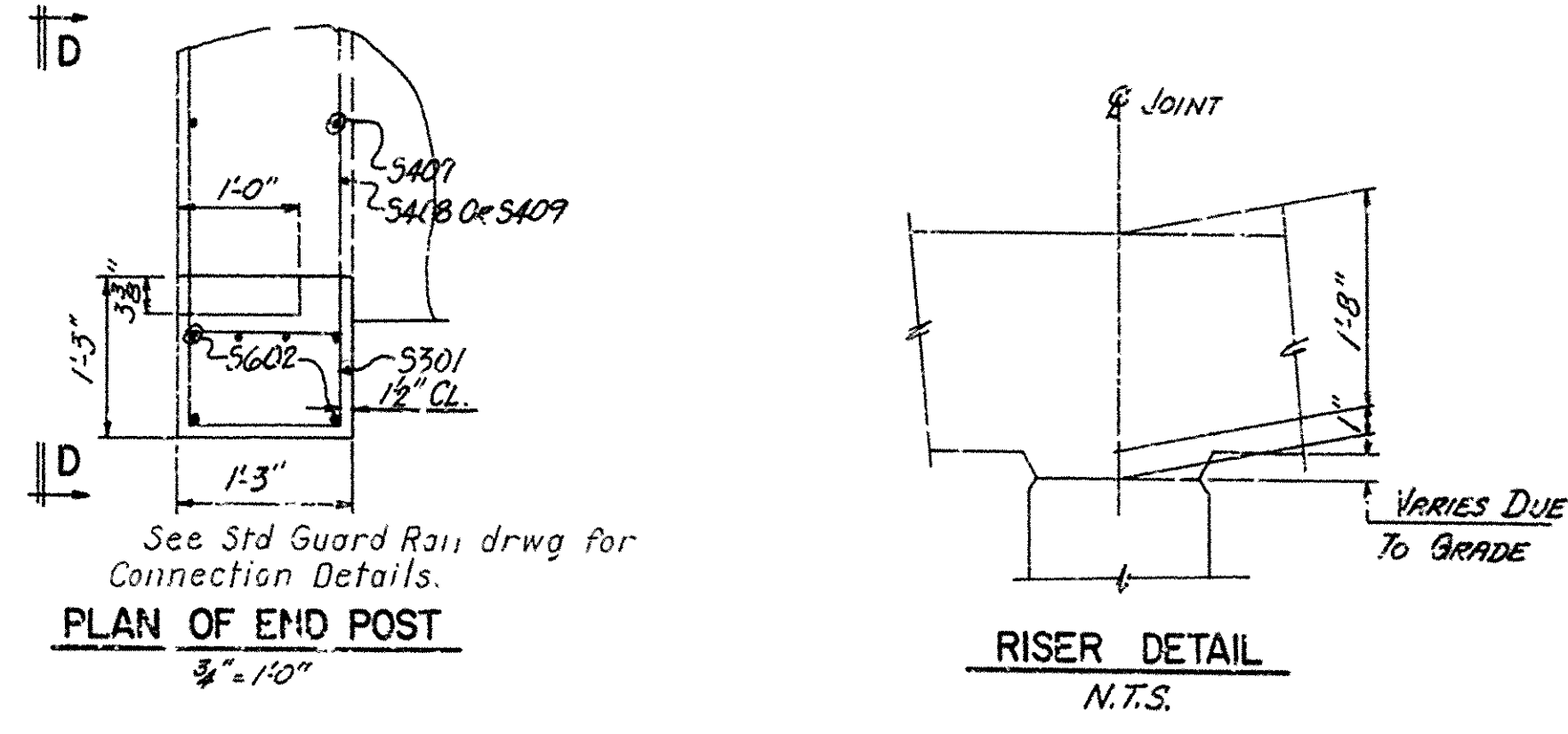
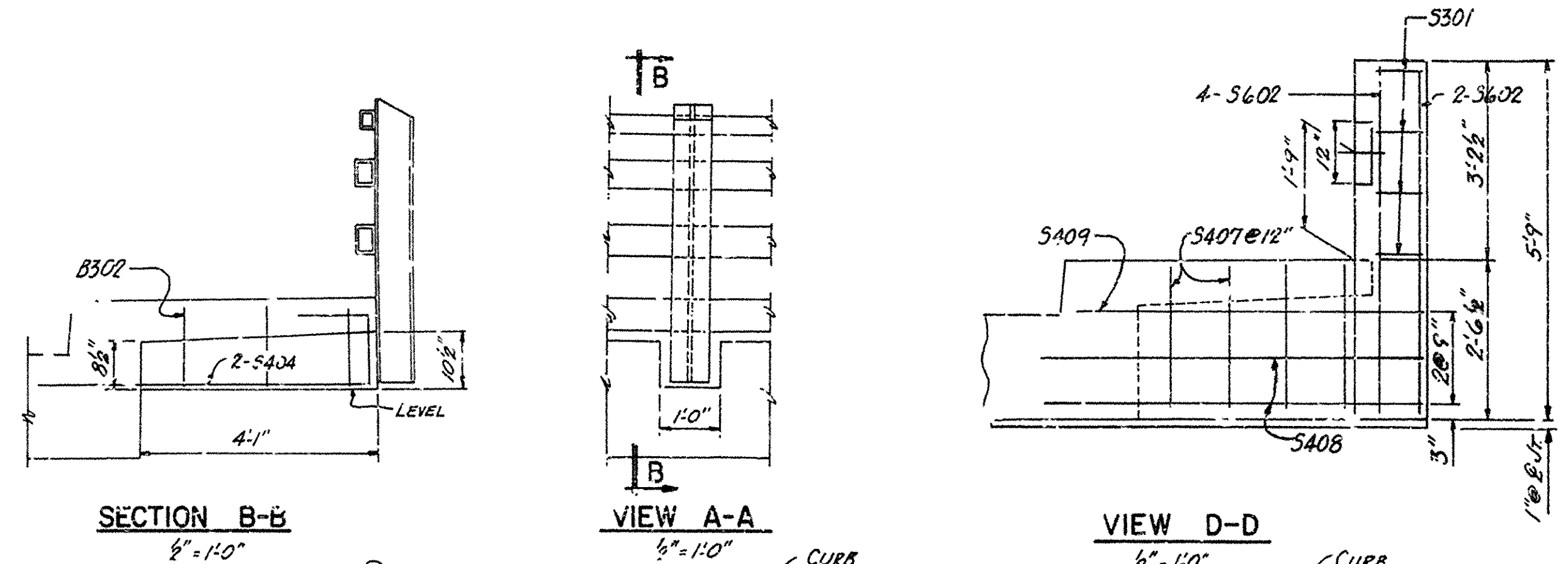
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 TRACED BY: \_\_\_\_\_ DATE: \_\_\_\_\_ SCALE: AS SHOWN  
 CHECKED BY: ELT DATE: 1-22-73

**BRIDGE NO. 424A** **DRAWING NO. 1817**

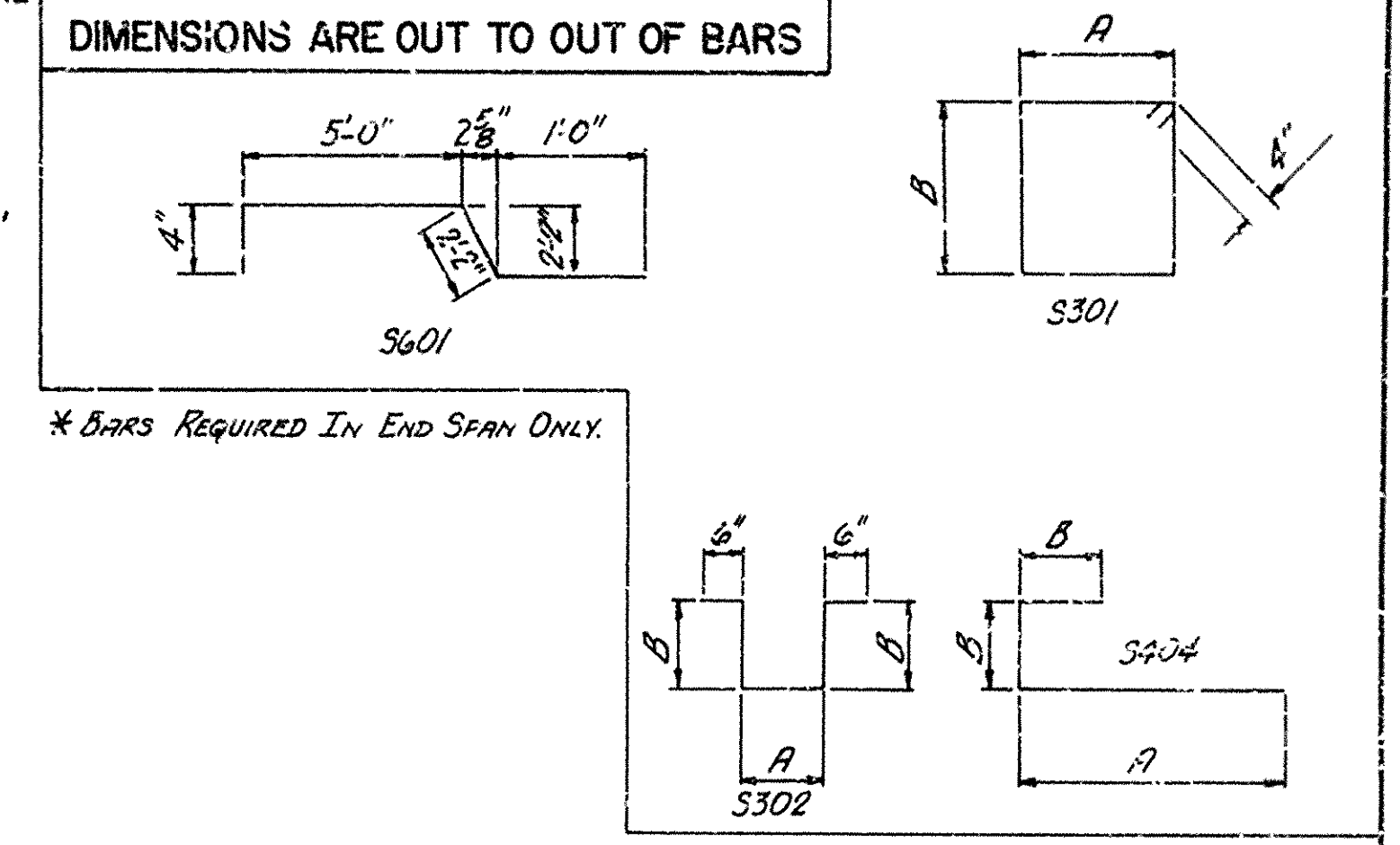
*Therel Pinkerton*  
BRIDGE ENGINEER

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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FEEDBACK NO.	STATE	FED. AID PROJ.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
				6	ARK.	5-132(9)		21	99
JOB NO. 4613									
S 424A SPAN DETAILS 13177									



BAR LIST FOR (ONE SPAN BOTH SIDES)									
MARK	NO. REQD.	LENGTH	A	B	PIN DIA.	BENDING DIAGRAM			
S1101	8	35'-10"			STR.	A			
S1001	34	35'-10"			STR.	S502			
S601	118	8'-1"	SEE DIAGRAM		3/2"	A			
S602	* 12	5'-5"			STR.	S401			
S501	144	8'-5"			STR.	S			
S502	114	3'-9"	3'-0"	10"	2 1/2"	S401			
S401	58	7'-6"	7'-0"	4"	3"	S			
S402	32	35'-10"			STR.	S			
S403	58	4'-10"			STR.	S			
S404	20	7'-8"	5'-6"	1'-2"	2"	S			
S405	* 6	4'-6"	8"	2'-0"	2"	S			
S406	* 4	2'-7"			STR.	S			
S407	16	2'-4"			STR.	S			
S408	* 4	13'-0"	1'-0"	6'-1"	2"	S			
S409	* 2	11'-10"	1'-0"	5'-6"	2"	S			
S301	* 8	5'-11"	8'-2"	1'-0"	1 1/2"	S405, S408, S409			
S302	30	3'-4"	8"	1'-0"	1 1/2"	S			



GENERAL NOTES

THE WORK CONTEMPLATED CONSISTS OF WIDENING THE EXISTING BRIDGE ON BOTH SIDES OF THE ROADWAY. FOR REQUIREMENTS IN CONDUCTING THE WORK, SEE JOB SPECIAL PROVISION "REMODELING EXISTING BRIDGE STRUCTURE". ALL DIMENSIONS RELATING TO EXISTING BRIDGE ARE TO BE VERIFIED IN THE FIELD AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING WIDENING TO EXISTING STRUCTURE.

ALL CONCRETE TO BE CLASS S (N=8). ALL EXPOSED CORNERS TO HAVE 3/4" CHAMFER UNLESS OTHERWISE NOTED.

REINFORCING STEEL TO BE ASTM A615, GRADE 40. REINFORCING STEEL IS TO BE HELD IN PLACE BY MEANS OF STEEL WIRE SUPPORTS SUFFICIENT IN NUMBER AND SIZE TO PROPERLY SUPPORT THE STEEL IN THE FORMS. THESE SHOP LISTS AND BENDING DIAGRAMS OF REINFORCING STEEL AND WIRE SUPPORTS MUST BE SUBMITTED AND APPROVAL SECURED BEFORE FABRICATION IS BEGUN.

FOR DETAILS OF BRIDGE RAILING, SEE DWG. 14993B. BRIDGE RAILING, RAILING POST AND FASTENINGS SHALL BE PAID FOR AT THE UNIT PRICE PER LINEAR FOOT BID FOR METAL BRIDGE RAILING.

ROOFING FELT, PREFORMED JOINT, AND POURED ASPHALT TO BE MEASURED AND PAID FOR AS CLASS S CONCRETE.

SPECIFICATIONS: ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 1972, AND APPLICABLE SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO 1969

LOADING: H20

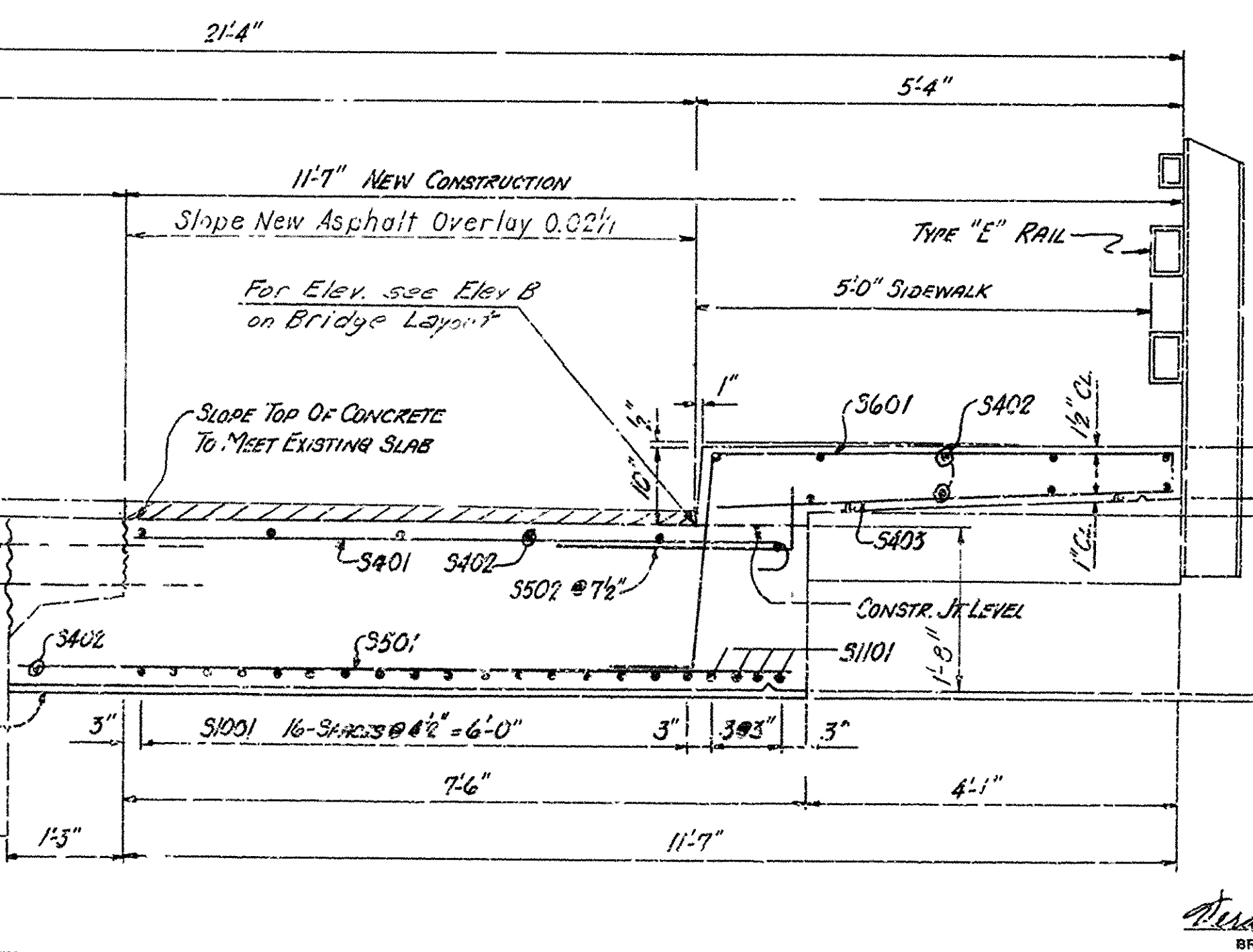
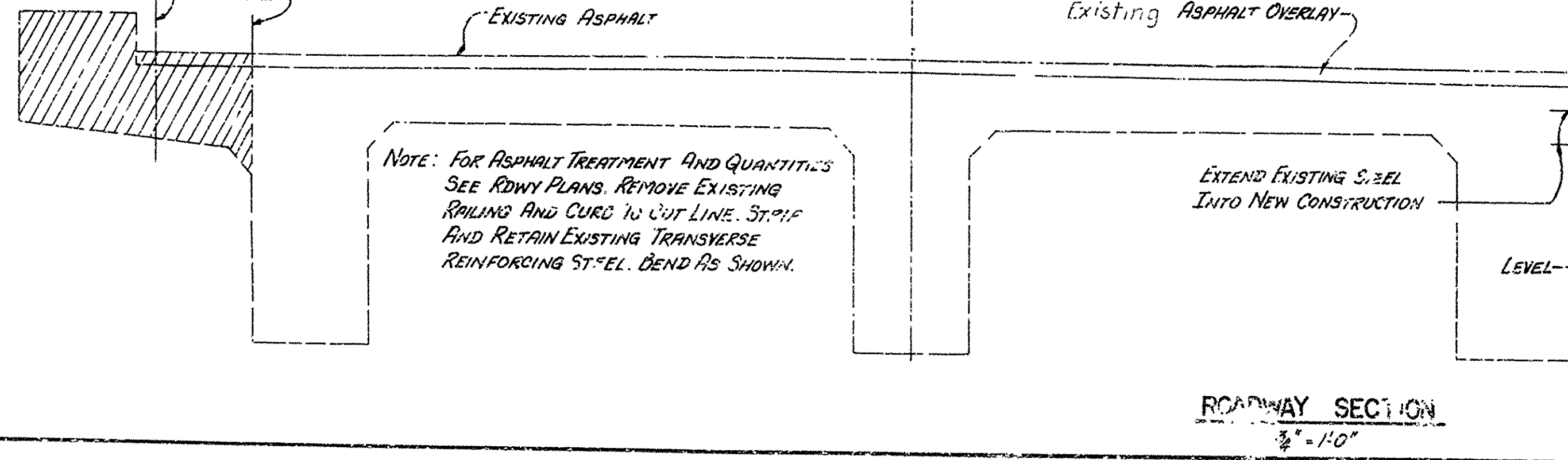
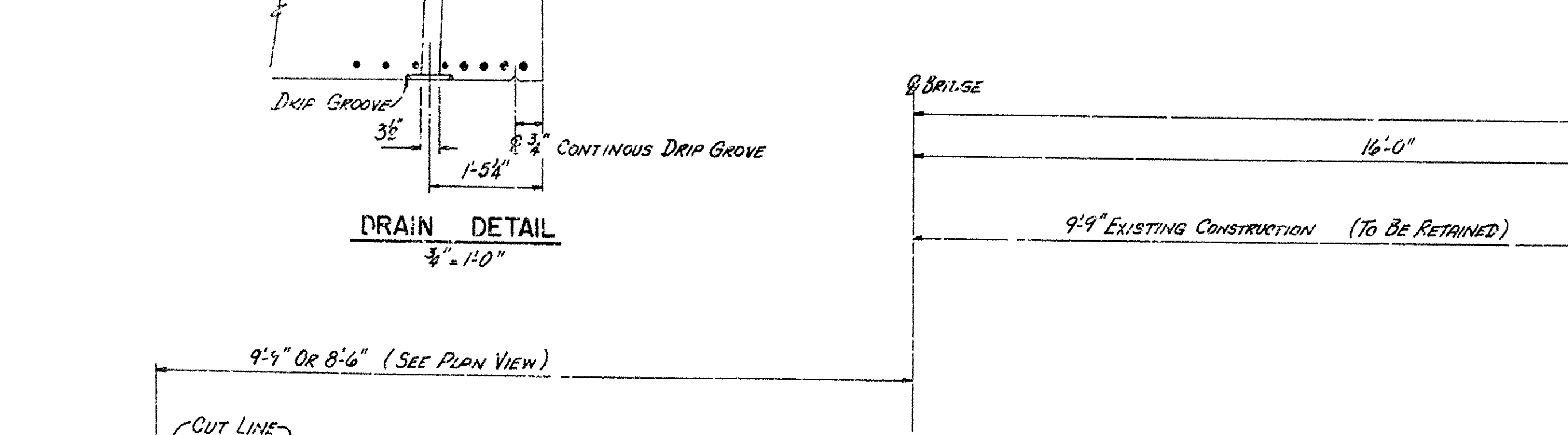
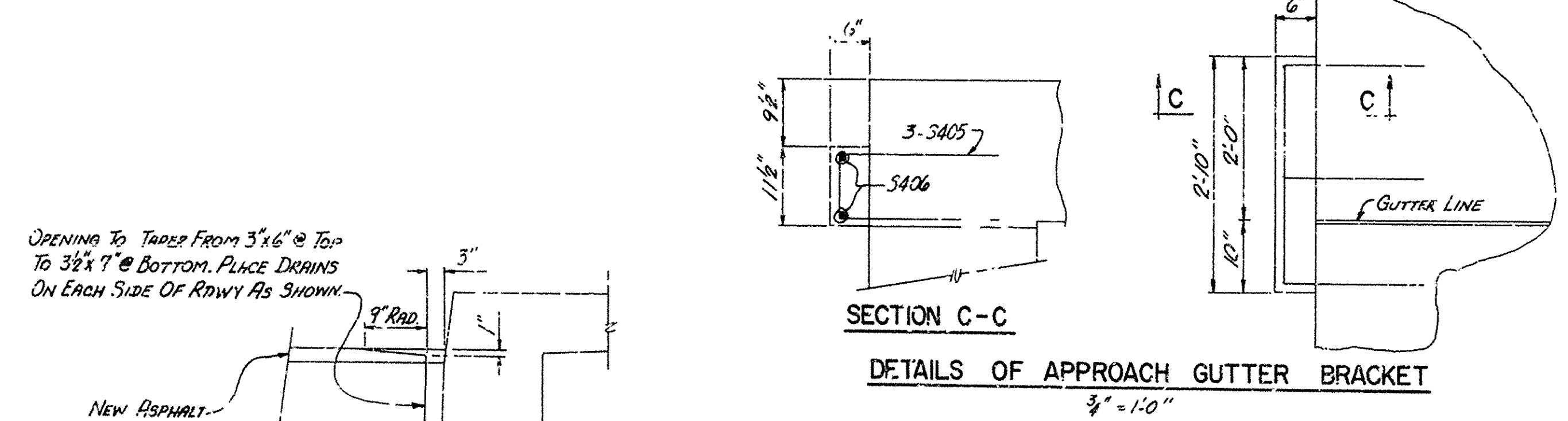
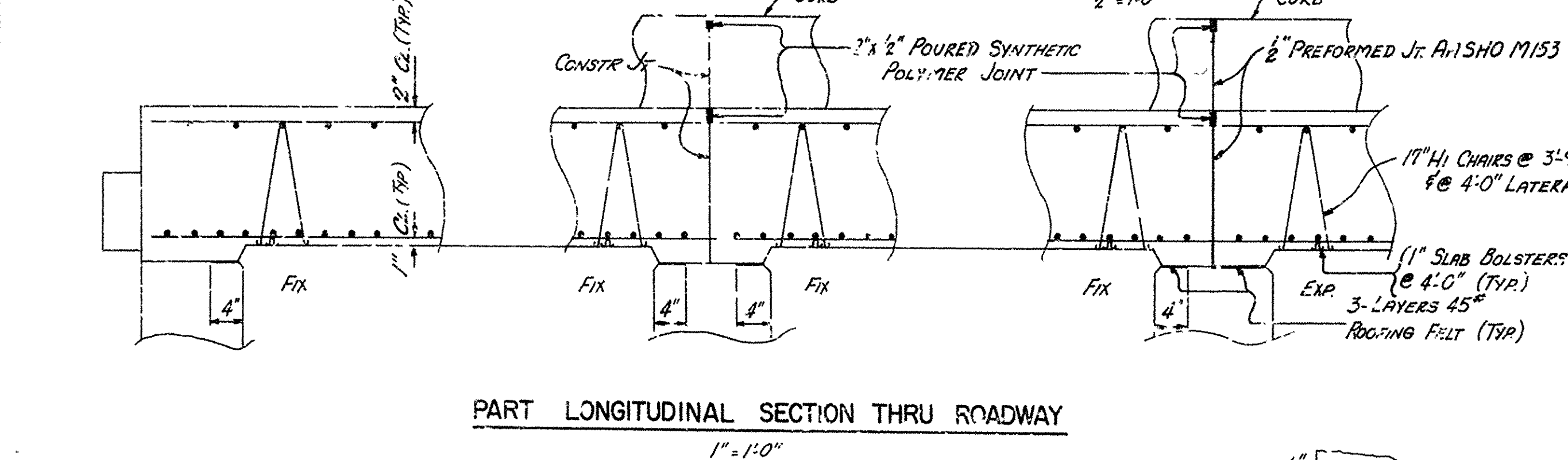
LOADING DISTRIBUTION TO SLAB:

DEAD LOAD - 356 PSF

LIVE LOAD - 0.164 WHEELS PER FT. OF WIDTH PLUS 30% IMPACT.

STRESSES: CLASS S CONCRETE (N=8) 1,600 PSI

REINFORCING STEEL 20,000 PSI



SHEET 2 OF 2

DETAIL OF WIDENING SPANS

HARTSELL CREEK

GREENWOOD LOGAN CO. LINE BRS. & APPRS

SEBASTIAN COUNTY

ROUTE 10 SEC. 1

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY: H.L. DATE: 1-16-73

TRACED BY: DATE: AS SHOWN

CHECKED BY: DATE: 1-27-73

BRIDGE NO. 424A DRAWING NO. 18177

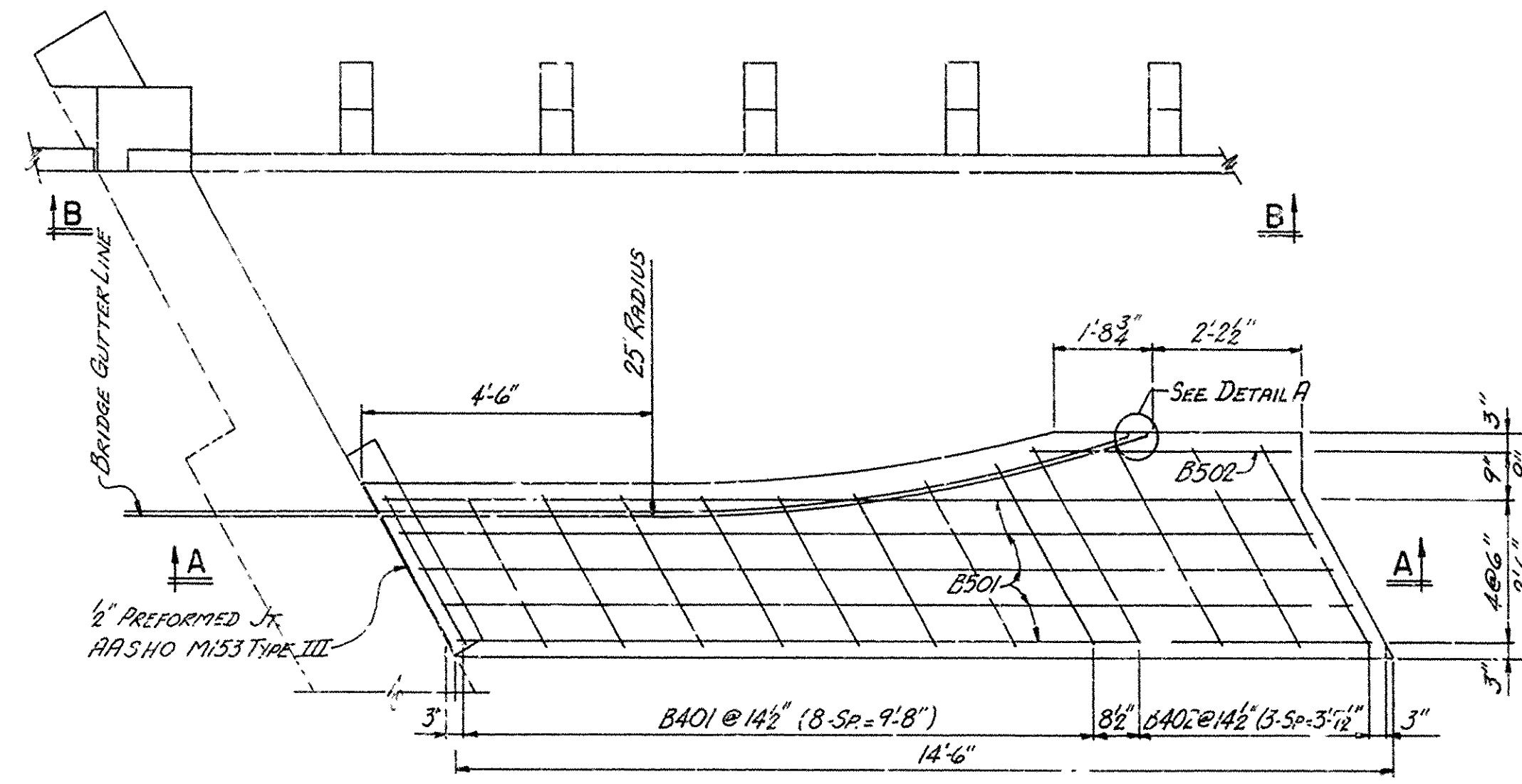


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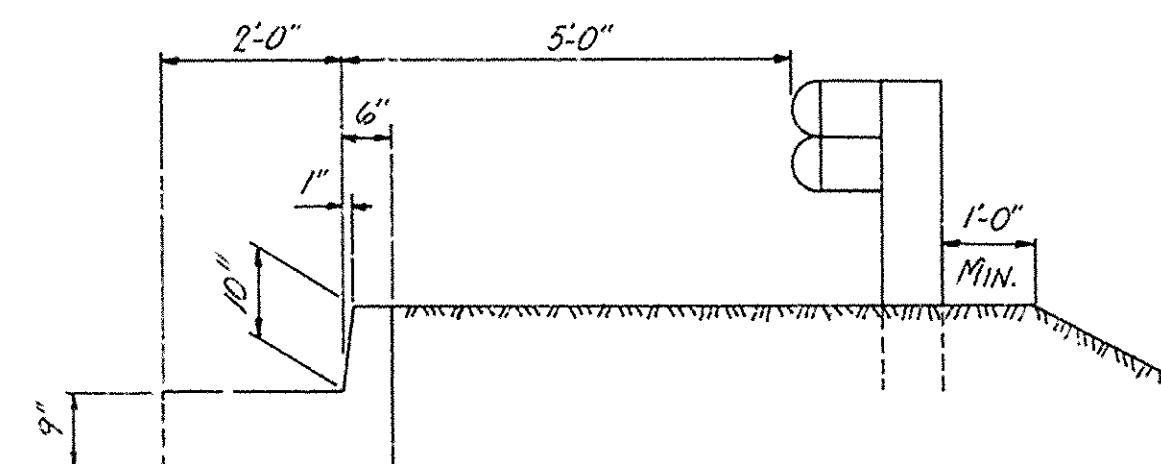
BAR LIST  
FOR ONE GUTTER  
BRIDGE 425A

MARK	NO REQD	LENGTH
B501	5	14'-0"
B502	1	4'-2"
B401	9	2'-4"
B402	4	3'-2"

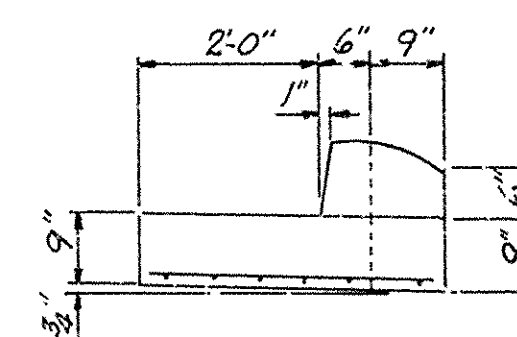
FOR DETAILS OF POSTS, GUARD FENCE AND ATTACHMENT OF GUARD FENCE TO POSTS, SEE  
JOB DETAILS PERTAINING TO THESE ITEMS



PLAN OF N.E. APPROACH GUTTER (ONE SIDE)  
FOR BRIDGE 425A



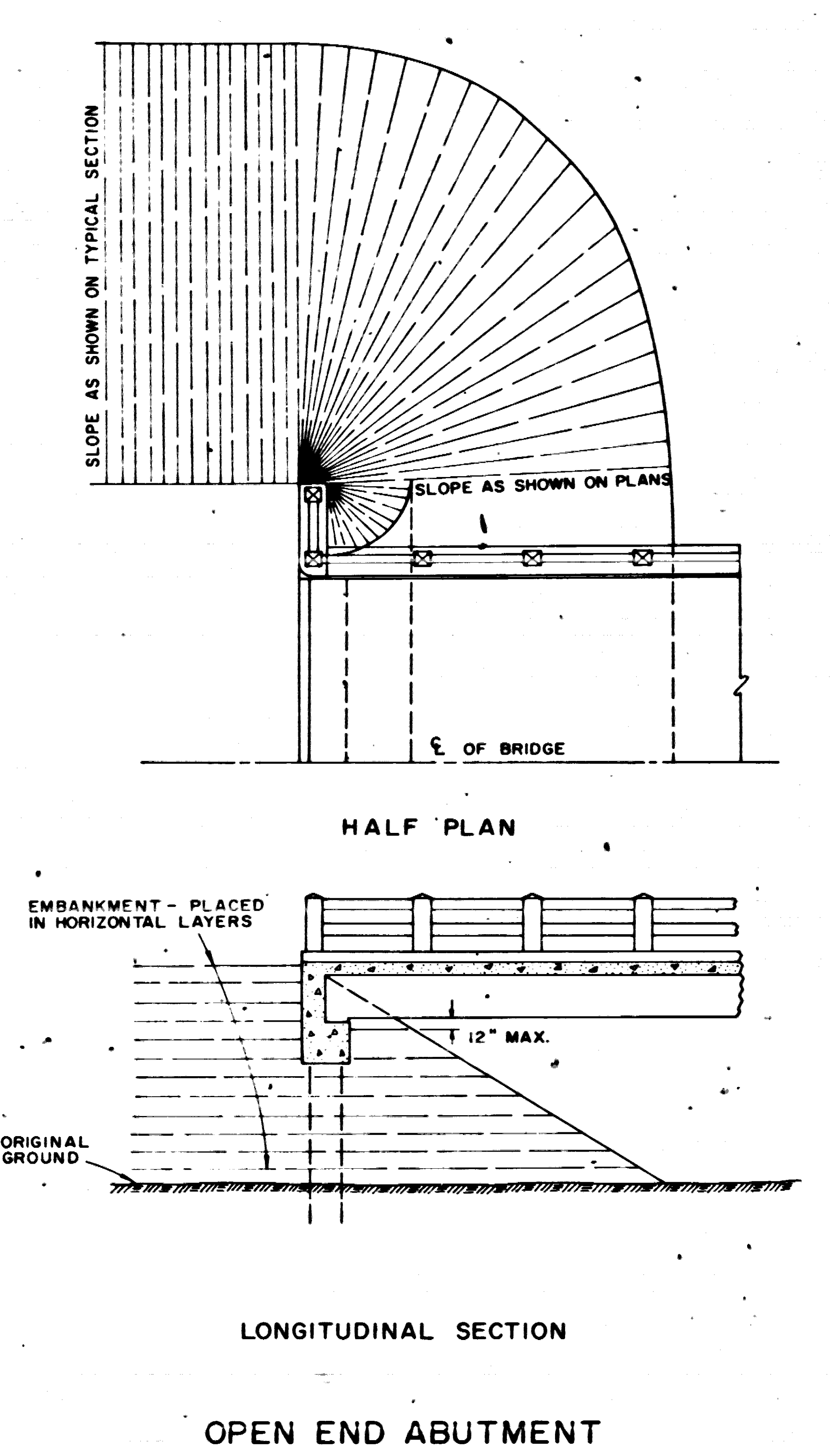
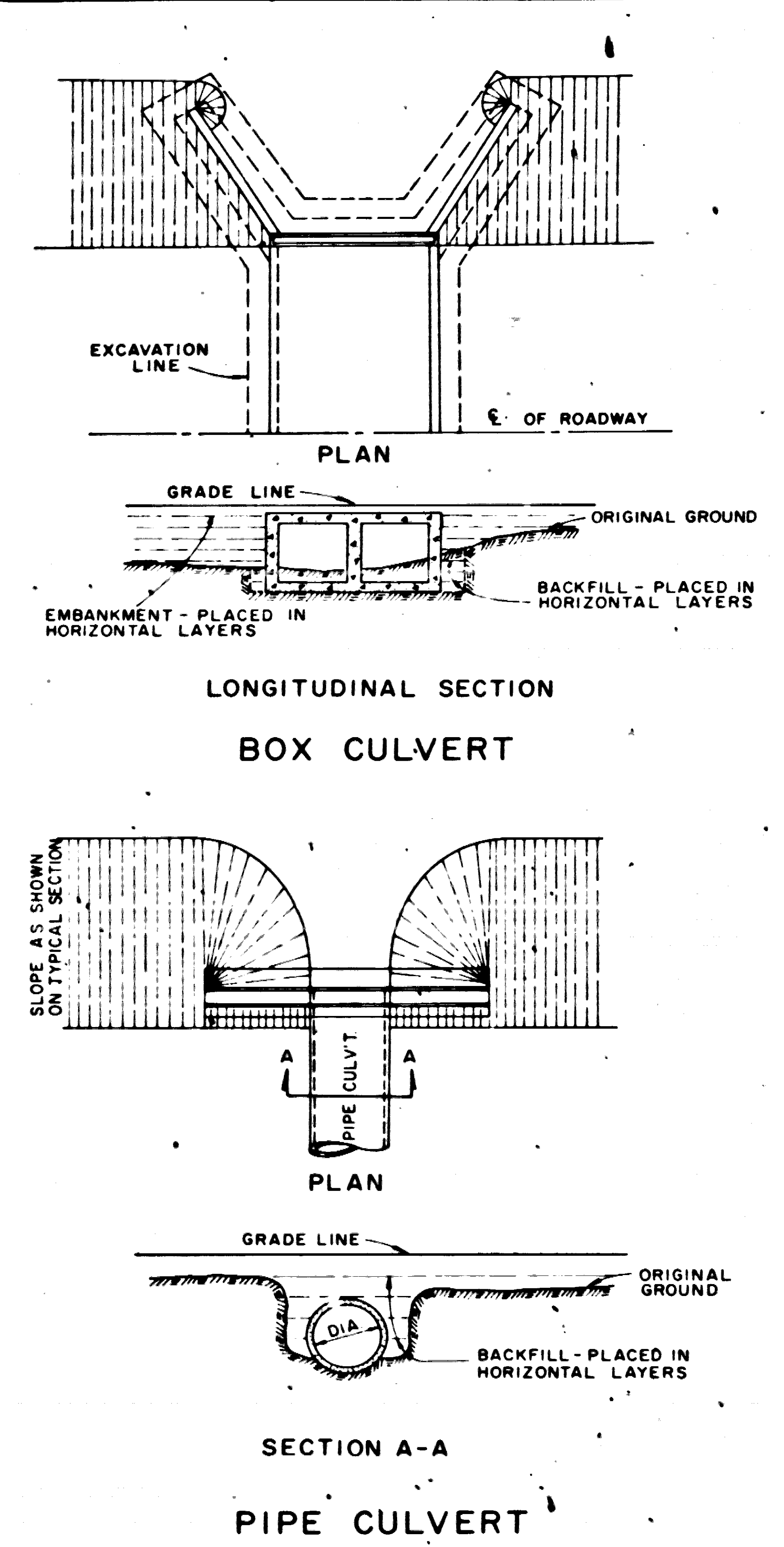
SECTION C-C



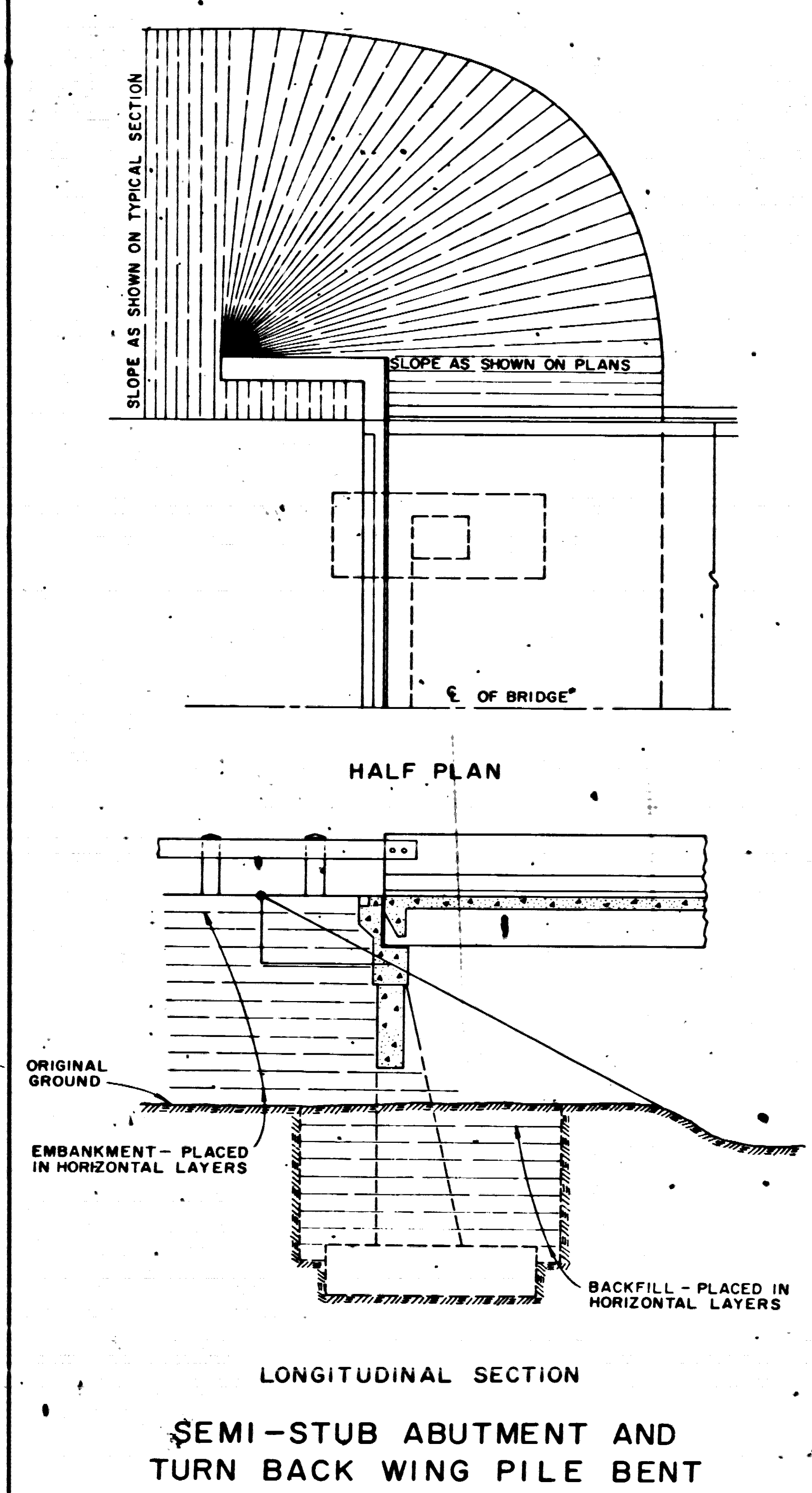
SECTION D-D

ARKANSAS STATE HIGHWAY COMMISSION  
LITTLE ROCK, ARK.

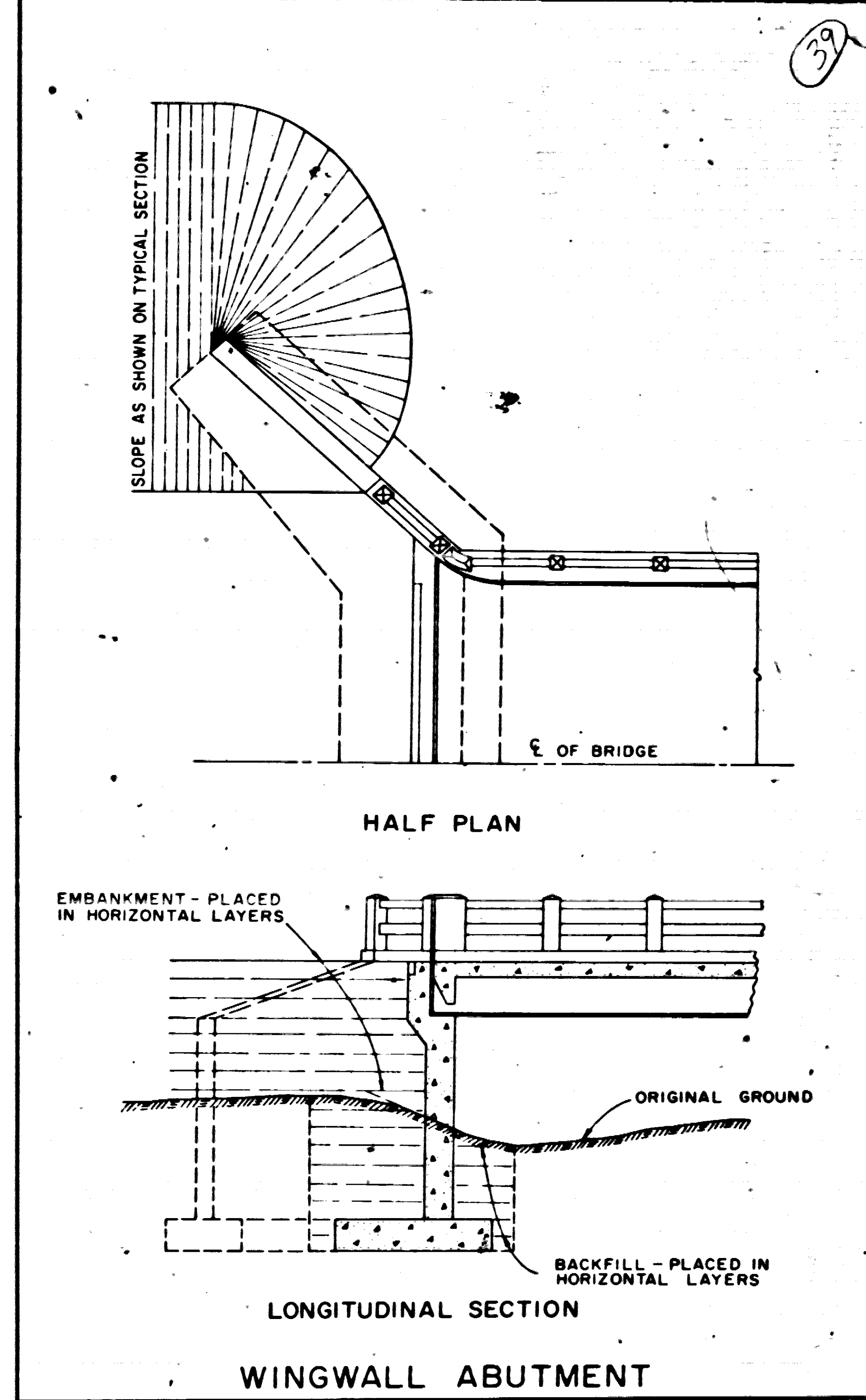
*Desal Pinkerton*  
BRIDGE ENGINEER



CONSTRUCTION OF THE BRIDGE END EMBANKMENT  
THE BRIDGE END EMBANKMENT SHALL BE DEFINED AS NOT LESS THAN 20 FEET OF EMBANKMENT ADJACENT TO THE END OF THE BRIDGE TOGETHER WITH THE SIDE SLOPES AND SLOPES UNDER THE BRIDGE END AND AROUND THE END OF WINGWALLS.  
REFER TO SUB-SECTIONS 210.08 AND 210.09 OF THE SPECIFICATIONS FOR CONSTRUCTION REQUIREMENTS.



BACKFILLING EXCAVATION  
IN SO FAR AS PRACTICABLE, ABUTMENT EXCAVATIONS SHALL BE CUT TO THE SIZE SHOWN ON THE PLANS WITH ALLOWANCE OF 3 FEET ON ALL SIDES.  
OVERSIZED AND FLARED CUTS TO AVOID THE USE OF SHEETING SHALL NOT BE PERMITTED.  
BACKFILL AROUND THE WALL OR COLUMNS SHALL BE COMPACTED IN ACCORDANCE WITH SUB-SECTION 801.08 OF THE SPECIFICATIONS.



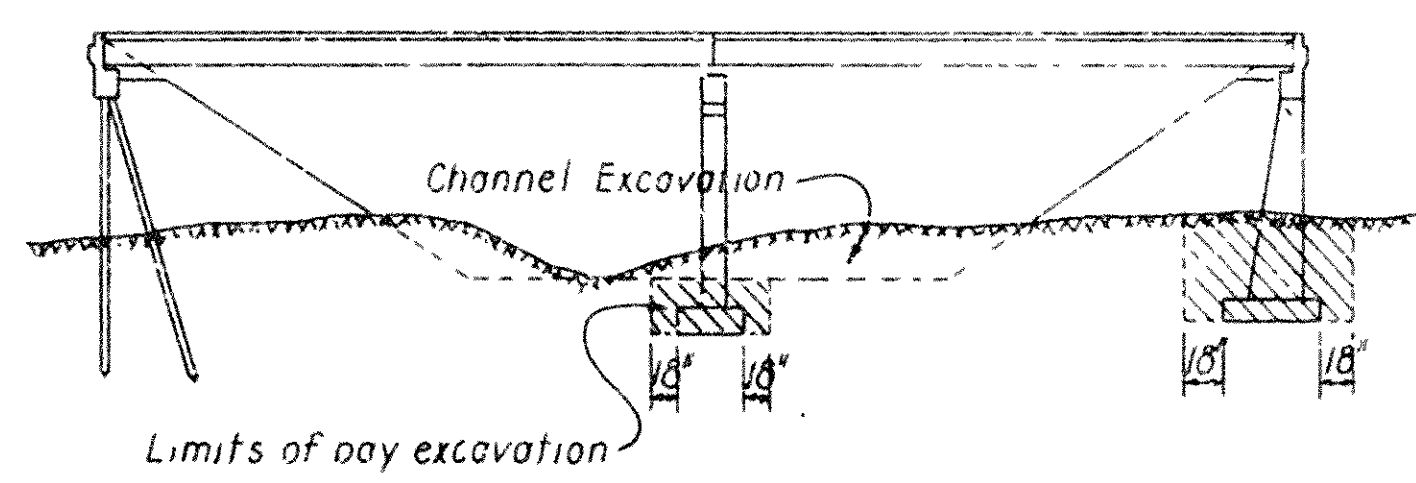
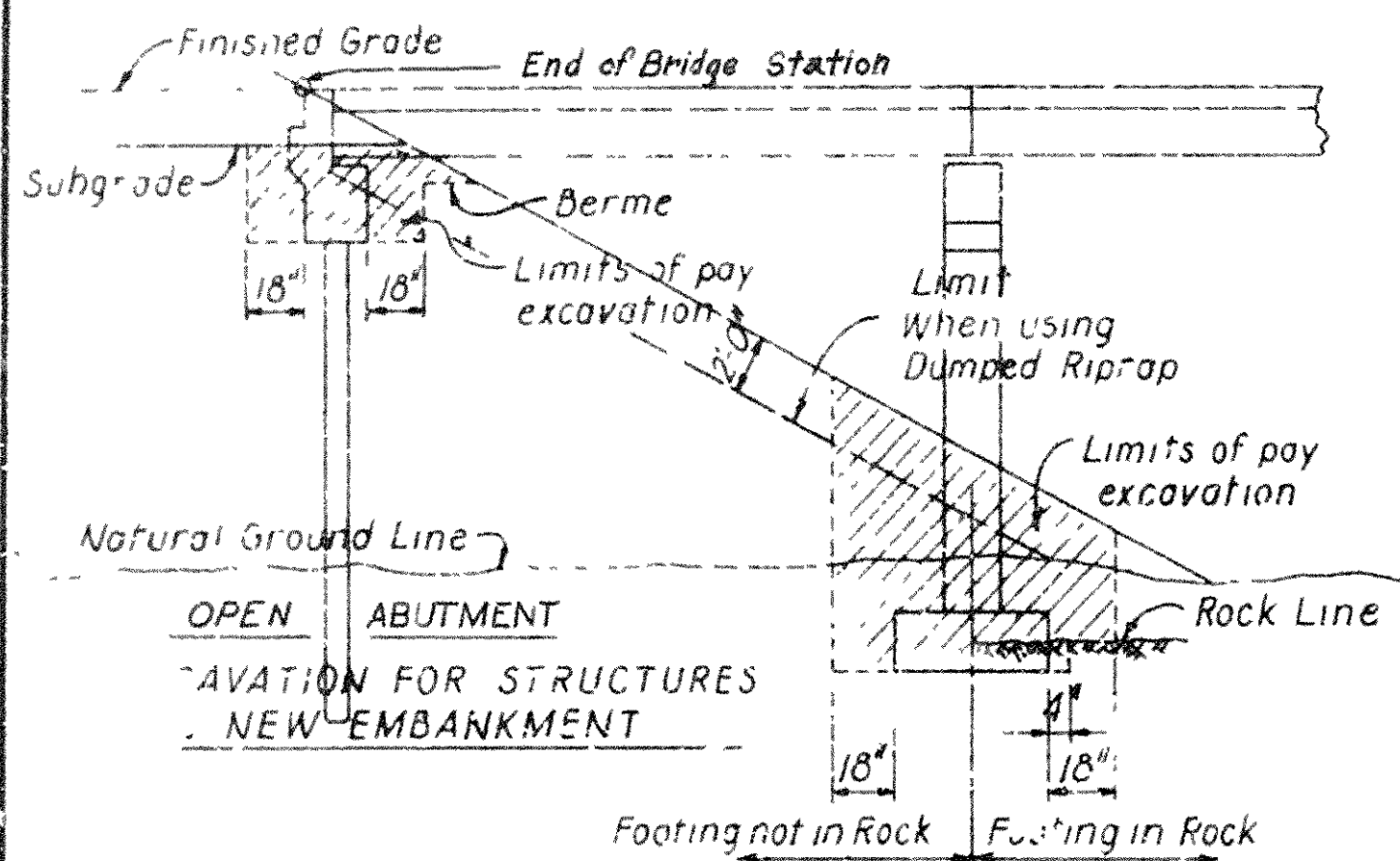
GENERAL NOTE  
BACKFILL AND EMBANKMENT ADJACENT TO STRUCTURES TO BE CONSTRUCTED IN 4 INCH HORIZONTAL LAYERS (LOOSE MEASURE) AND COMPACTED TO THE SATISFACTION OF THE ENGINEER BY USE OF MECHANICAL EQUIPMENT.

ARKANSAS STATE HIGHWAY COMMISSION		
EMBANKMENT CONSTRUCTION AT BRIDGE ENDS AND BACKFILL FOR STRUCTURES		
STANDARD DRAWING 1888A		
9-15-78	SECTION 202 TO 210	9-15-78
10-2-72	REVISED & REDRAWN	10-2-72
DATE	REVISION	DATE FILMED

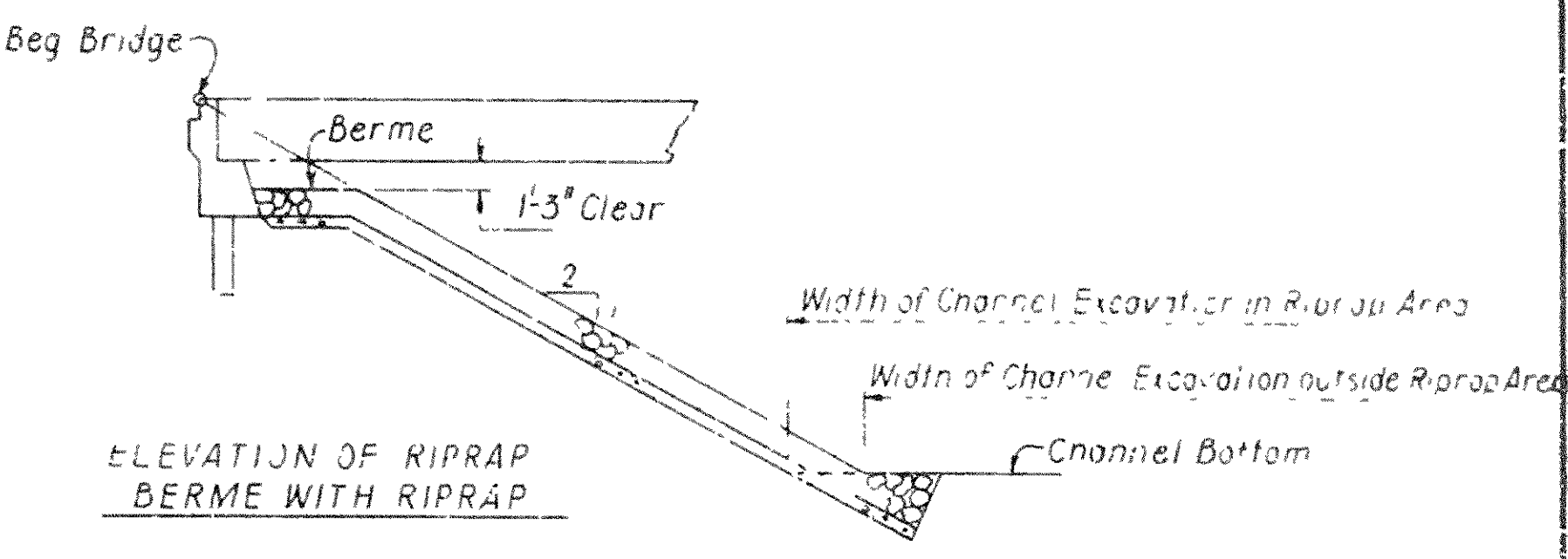
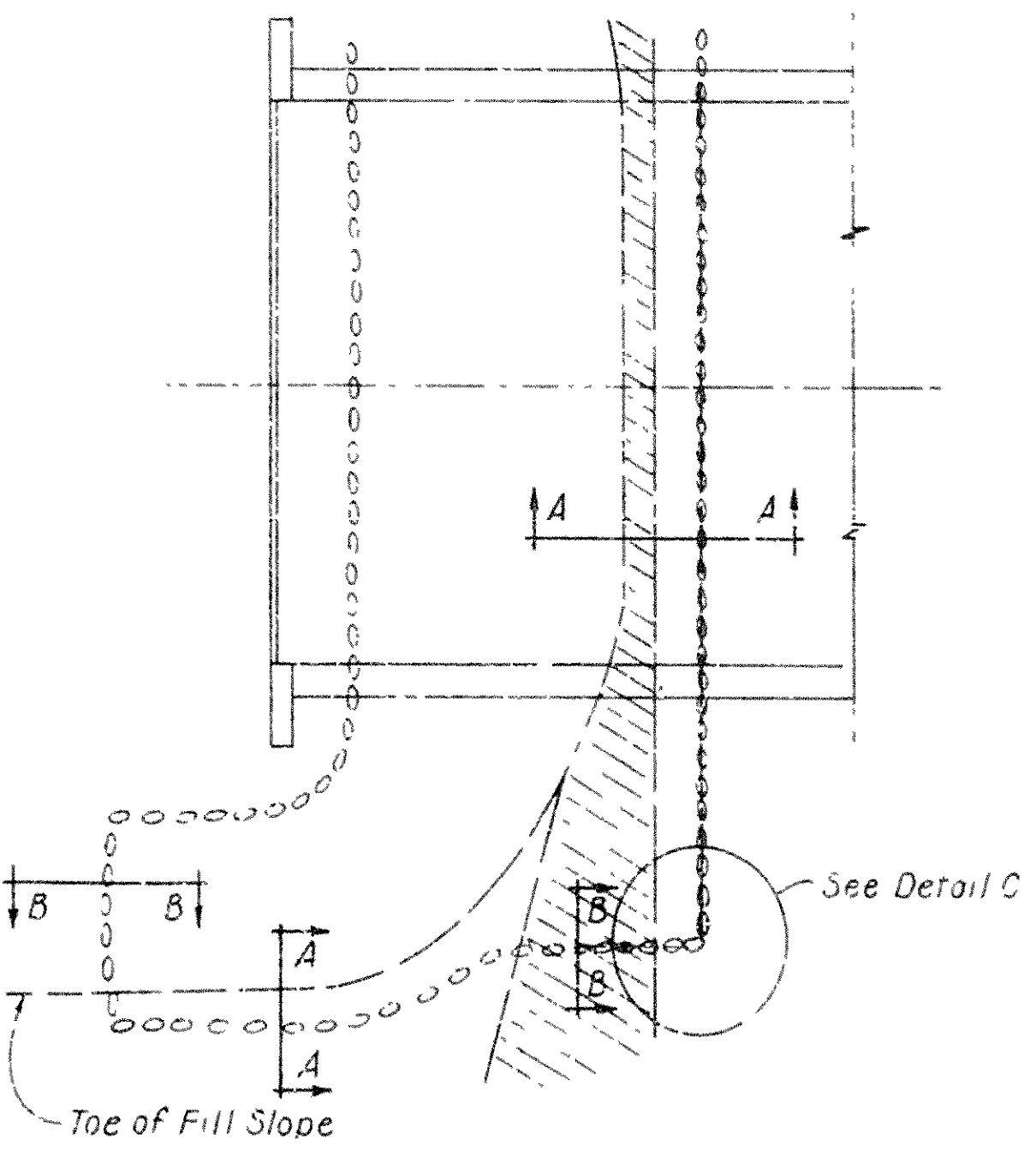


DATE	DATE	DATE	DATE	FILE NO.	STATE	FILE NO.	FILE NO.	FILE NO.	FILE NO.	FILE NO.
2/16/72	503-2-24-72				ARK					

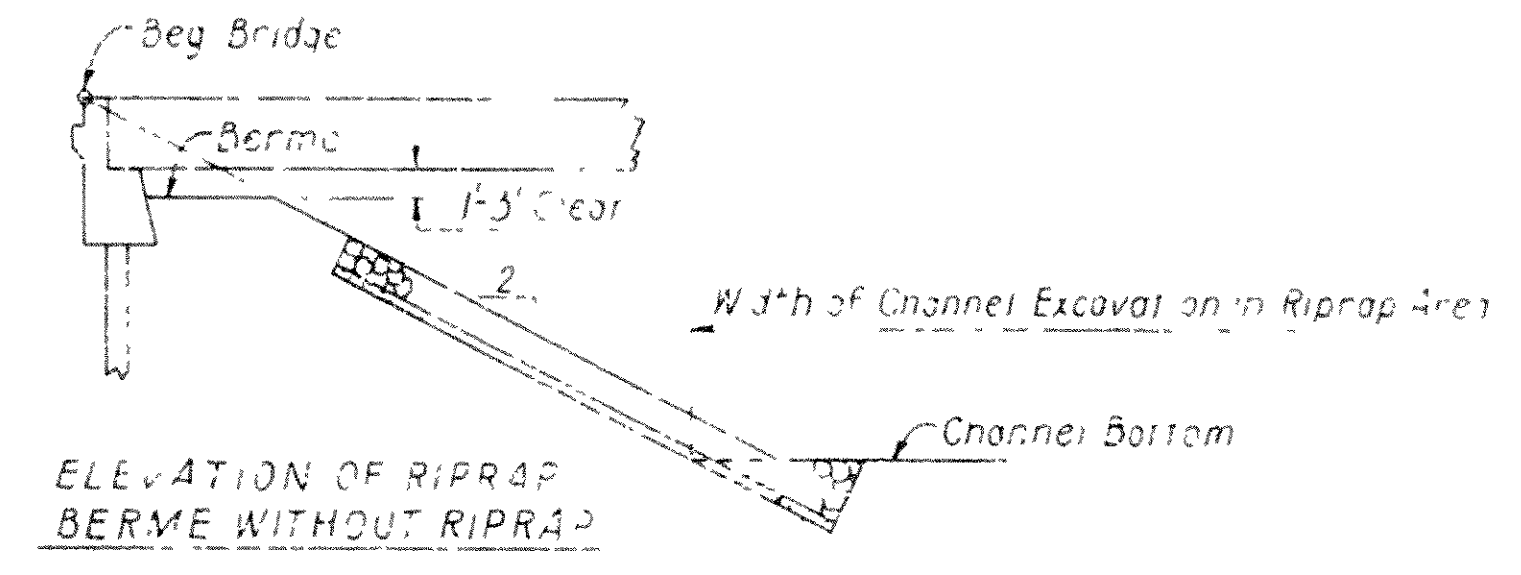
① Riprap Details & Excavation 1891E



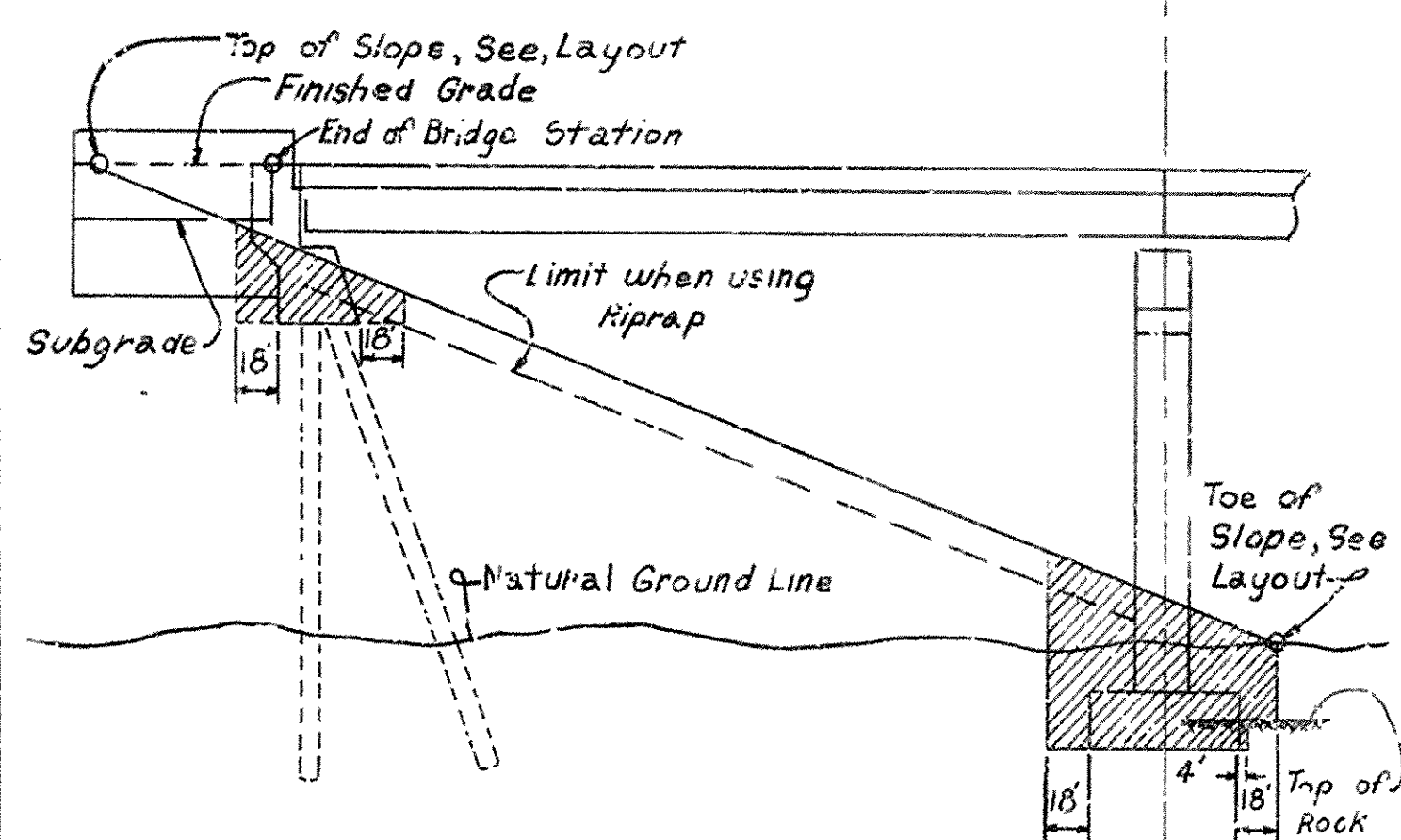
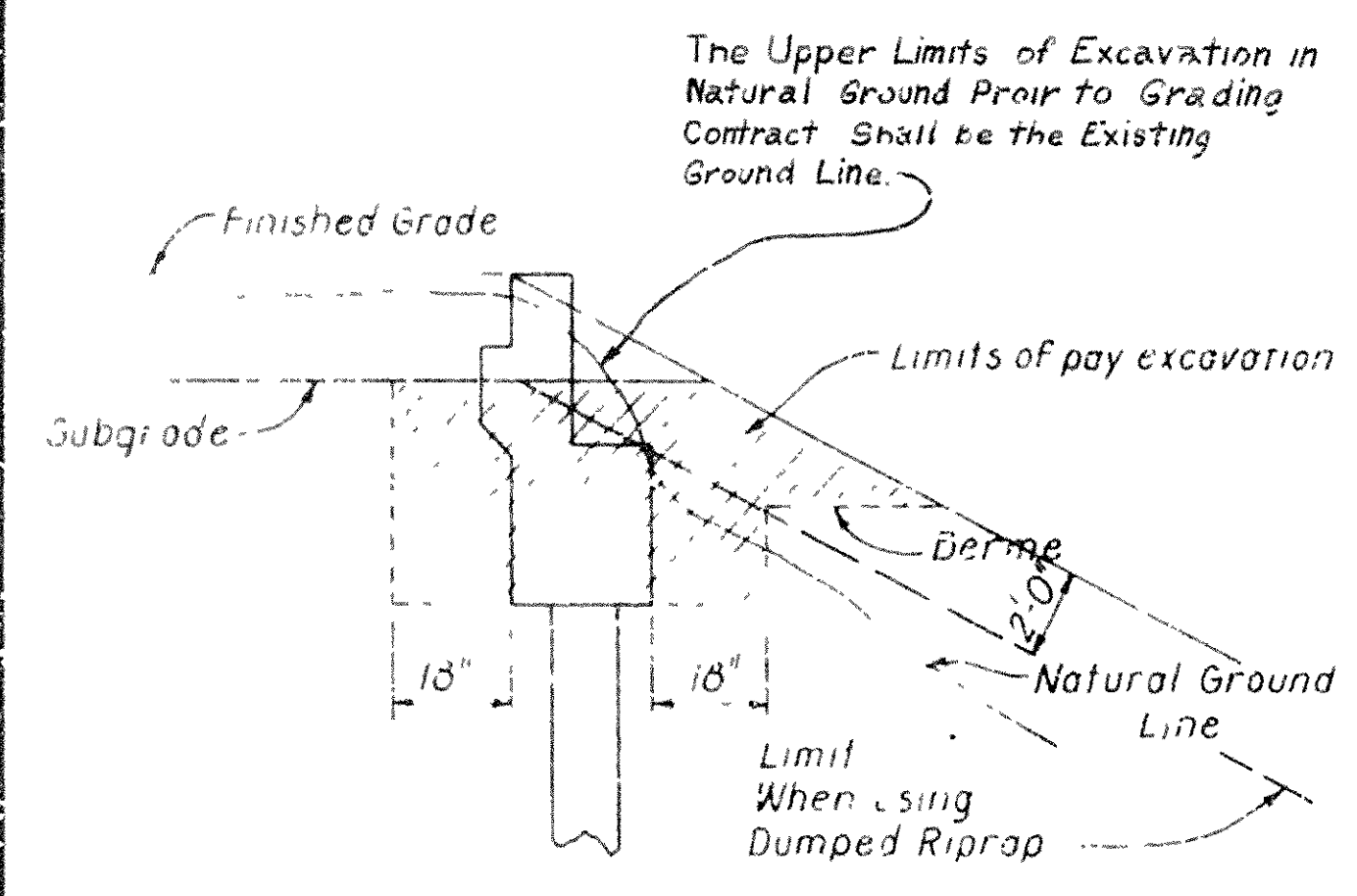
EXCAVATION FOR STRUCTURES  
BRIDGE LOCATION WITH DESIGNATED CHANNEL CHANGE



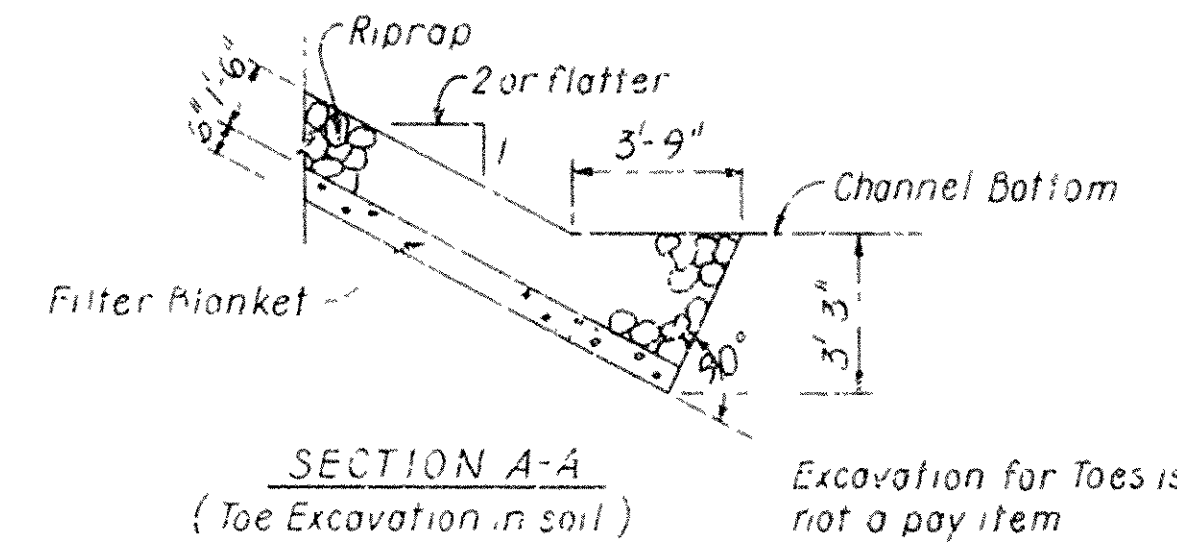
PLAN OF DUMPED RIPRAP



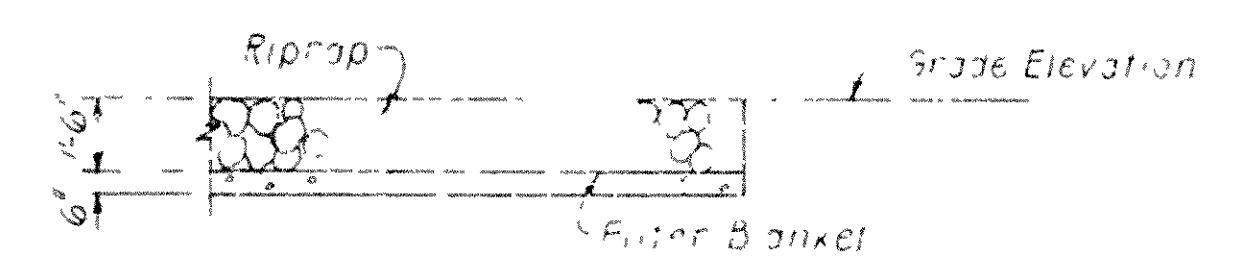
ELEVATION OF RIPRAP  
BERME WITHOUT RIPRAP



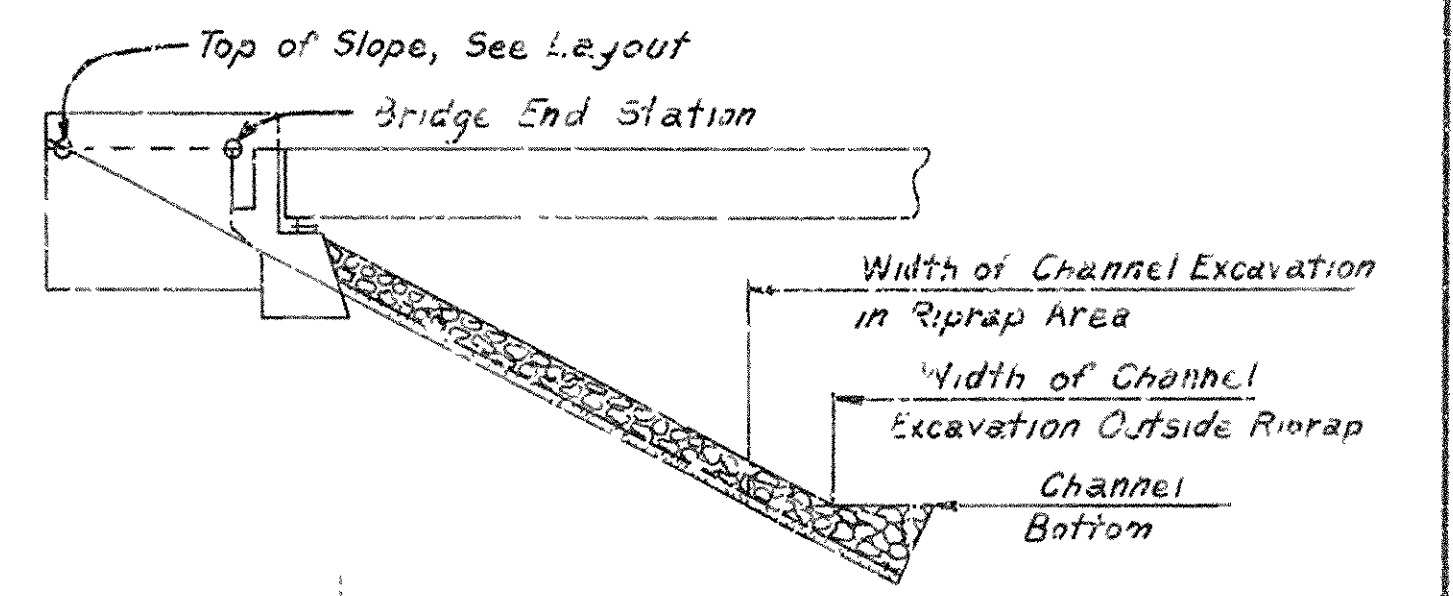
SEMI-STUB ABUTMENT  
Excavation For Structures  
in New Embankment



SECTION A-A  
(Toe Excavation in soil)

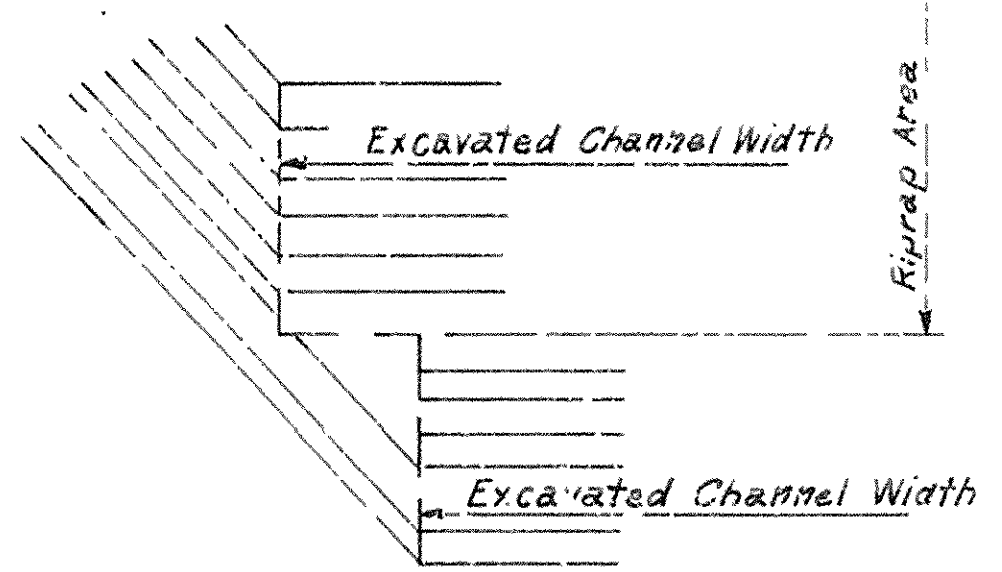


SECTION B-B

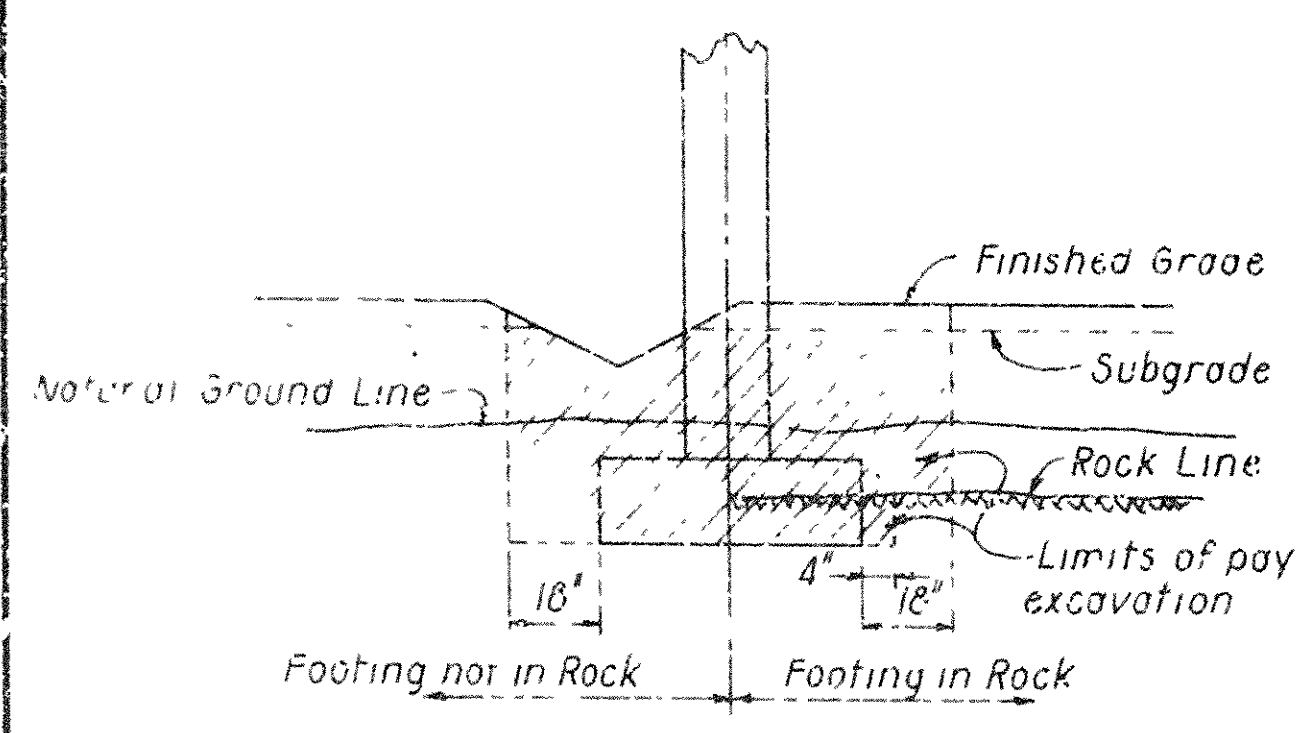


SECTION A-A  
(Toe Excavation in Rock)

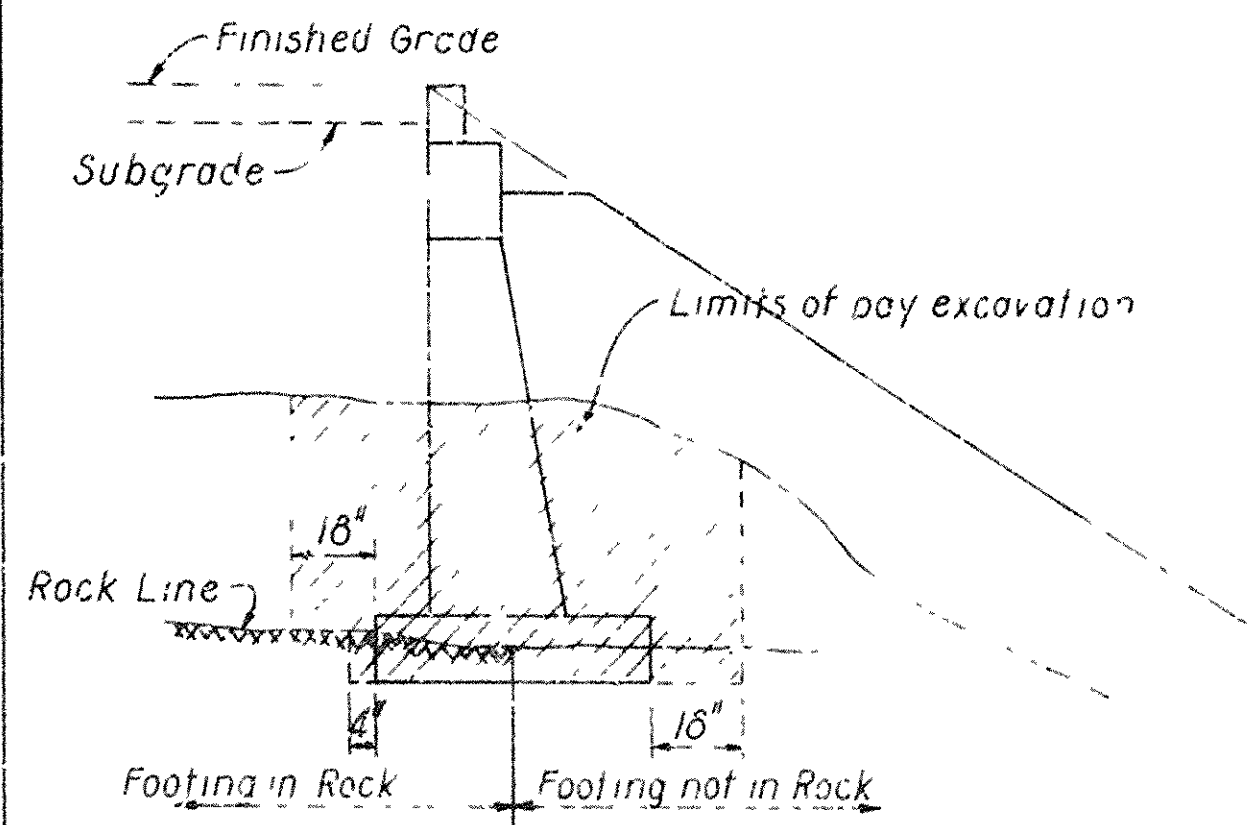
Note: Use this type of toe when rock is encountered which is in a stable condition



DETAIL C



EXCAVATION FOR STRUCTURES  
IN ROADWAY FILL SECTION



EXCAVATION FOR STRUCTURES  
IN NATURAL GROUND AND NEW EMBANKMENT

NOTE: DETAILS FOR COMPUTING EXCAVATION FOR STRUCTURES ARE INCLUDED FOR INFORMATION AS TO HOW PLAN QUANTITIES WERE CALCULATED AND FOR USE WHEN ADJUSTING QUANTITIES WHEN CHANGING FOOTING ELEVATION.

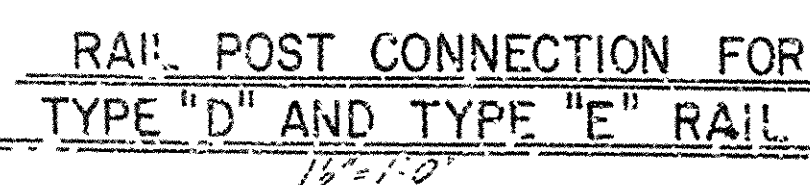
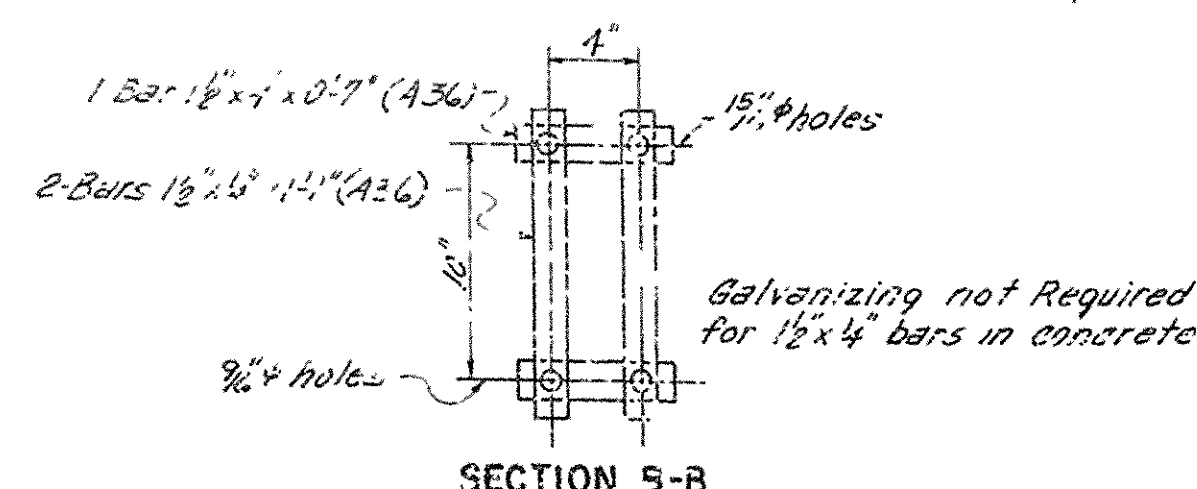
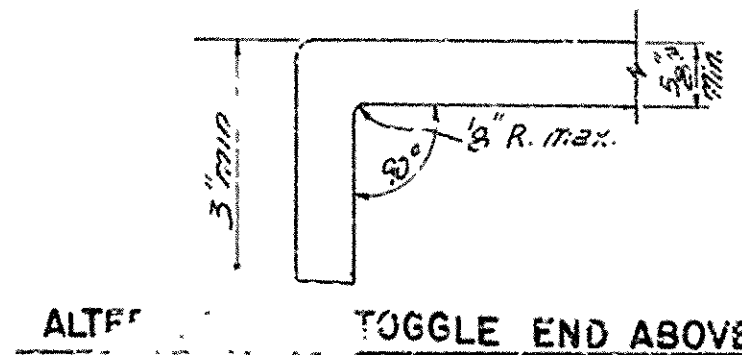
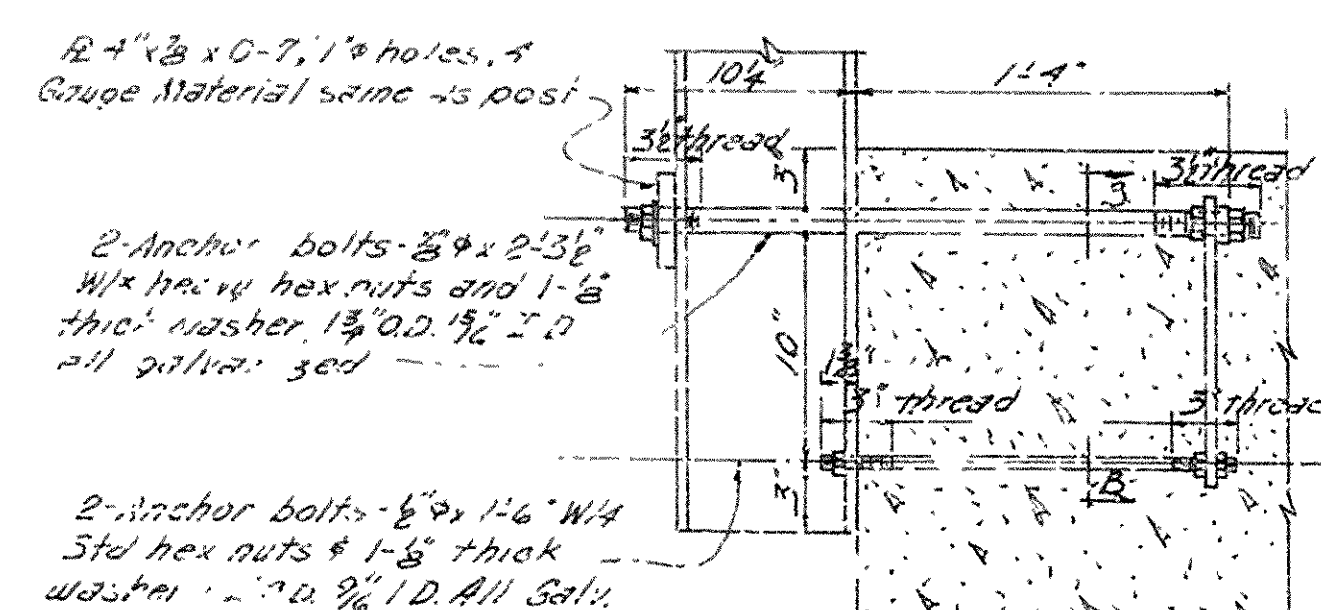
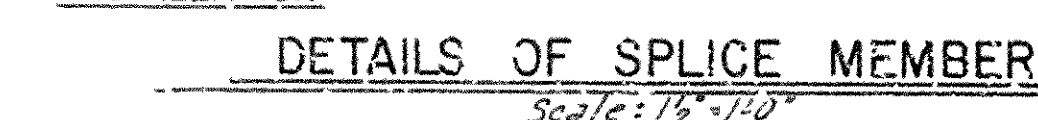
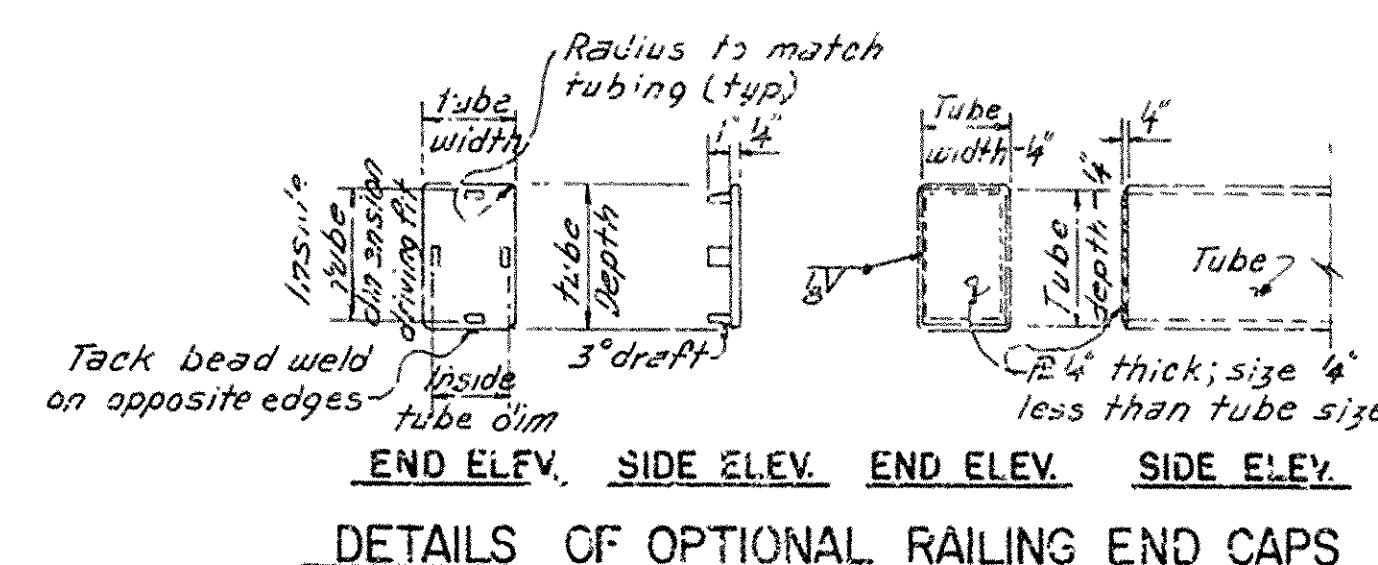
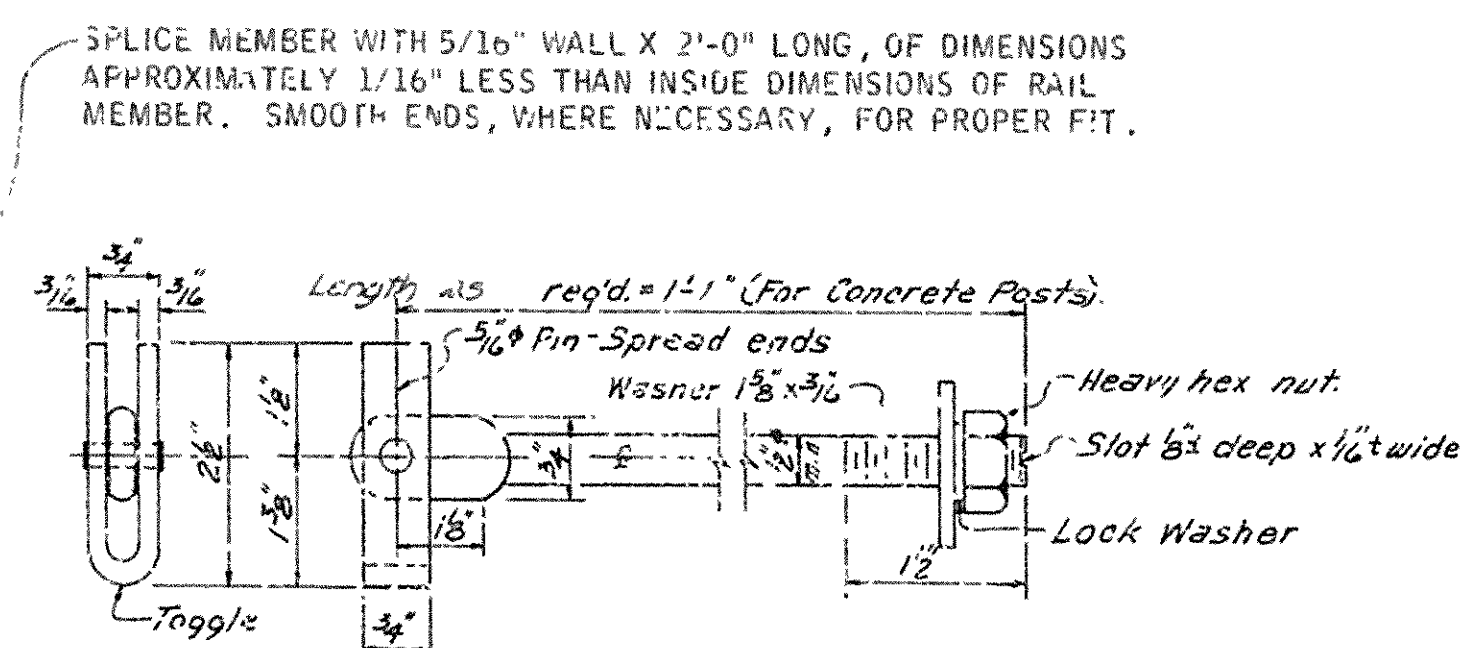
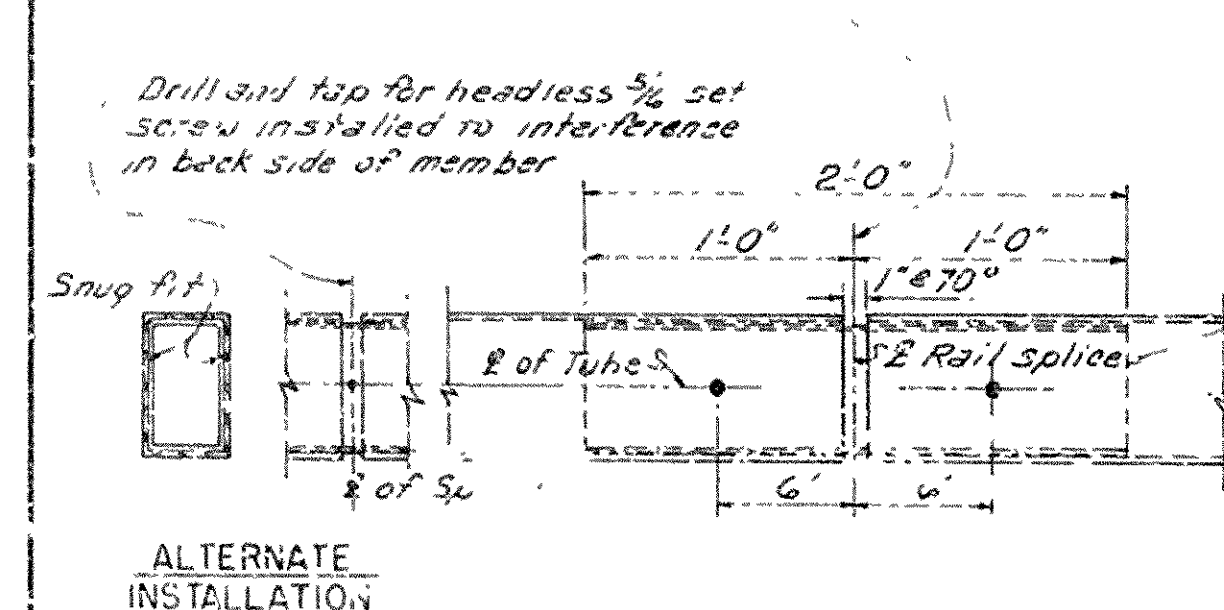
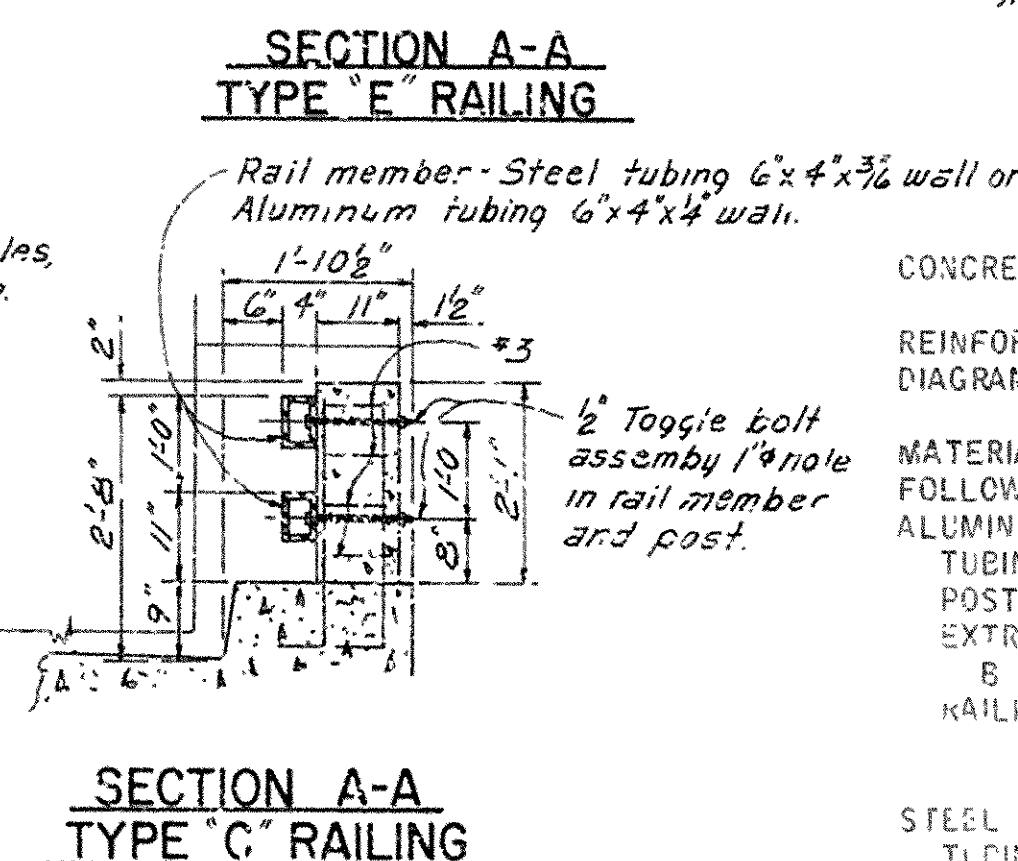
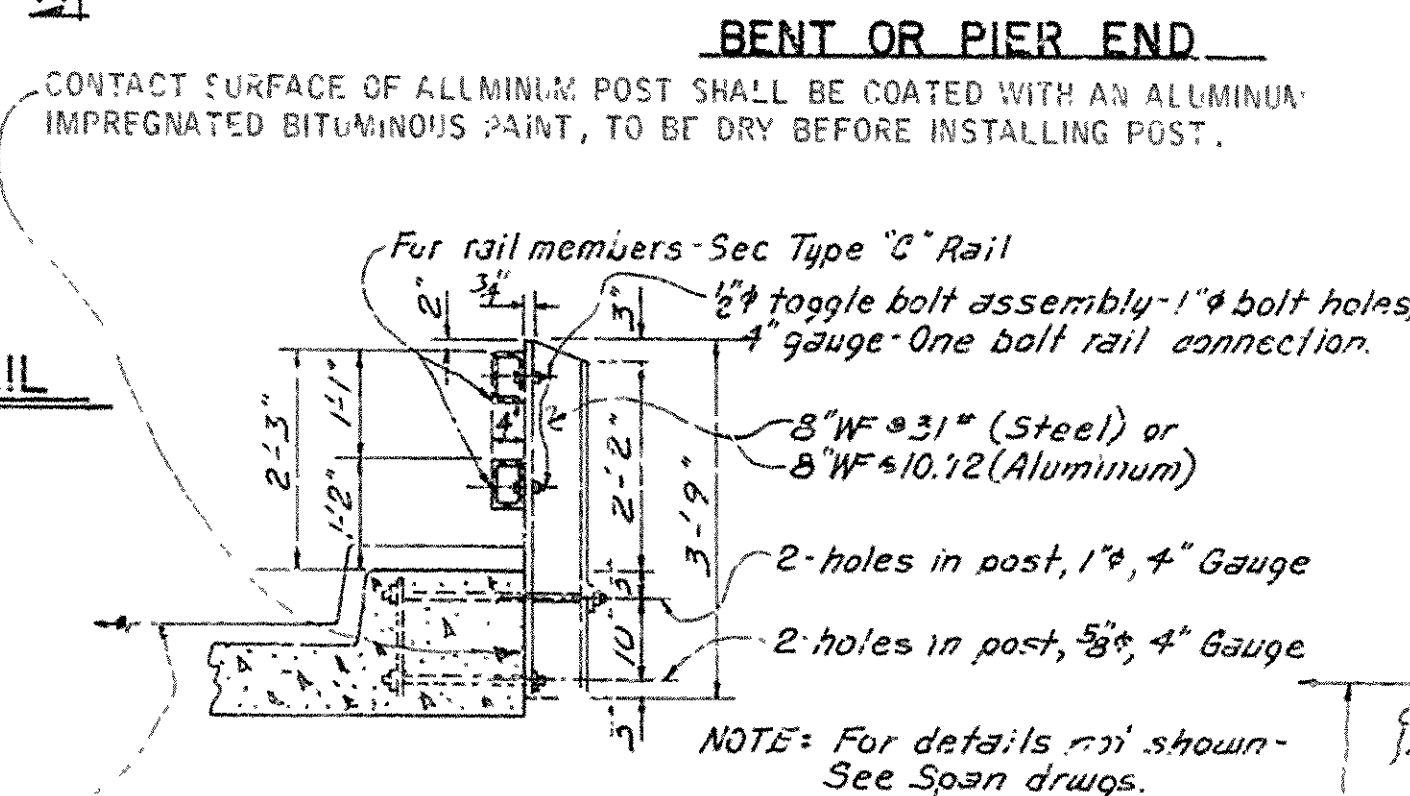
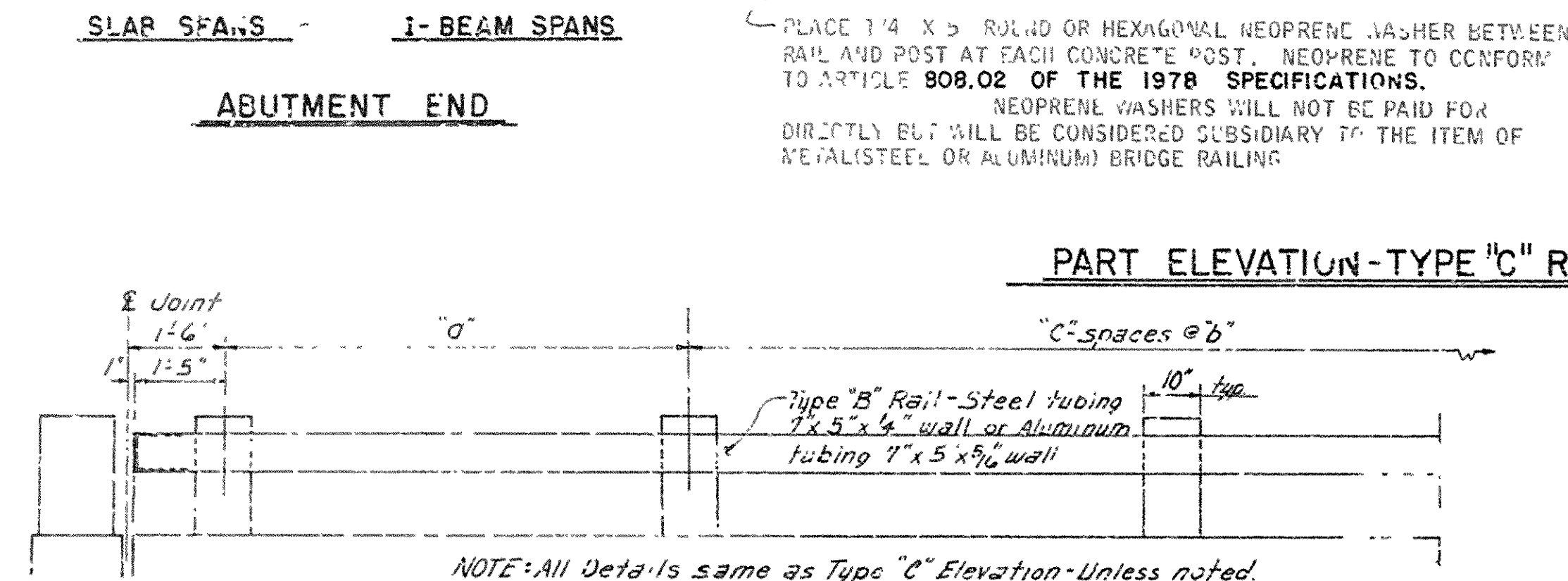
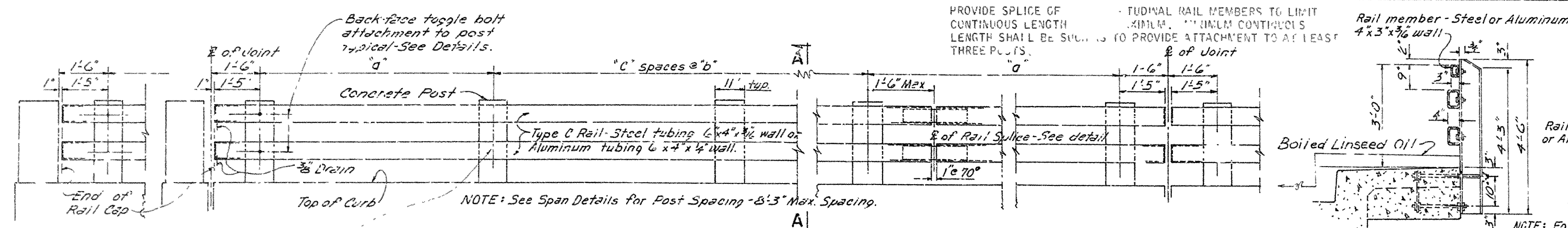
NOTE: Drawing Adapted From Drawing No. 111D With The SEMI-STUB ABUTMENT Detail Added. L.E.G. 2/16/72

DETAILS FOR DUMPED RIPRAP  
AND FILTER BLANKET AND  
DETAILS FOR COMPUTING  
EXCAVATION FOR STRUCTURES  
ROUTE SEC.  
ARKANSAS STATE HIGHWAY COMMISSION  
LITTLE ROCK, ARK.

DRAWN BY: *W. H. H.* DATE: 4-16-70  
TRACED BY: *F.M.H.* DATE: 4-20-70  
CHECKED BY: *F.M.H.* DATE: 4-20-70  
BRIDGE NO. *1891E* DRAWING NO. 1891E



DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. ROAD NO.	STAT.	FED. AID PROJ.	FISCAL YEAR	BELT NO.	TOTAL SHEET
9-15-78	689-62184			6	ARK.				
				JOB NO.					



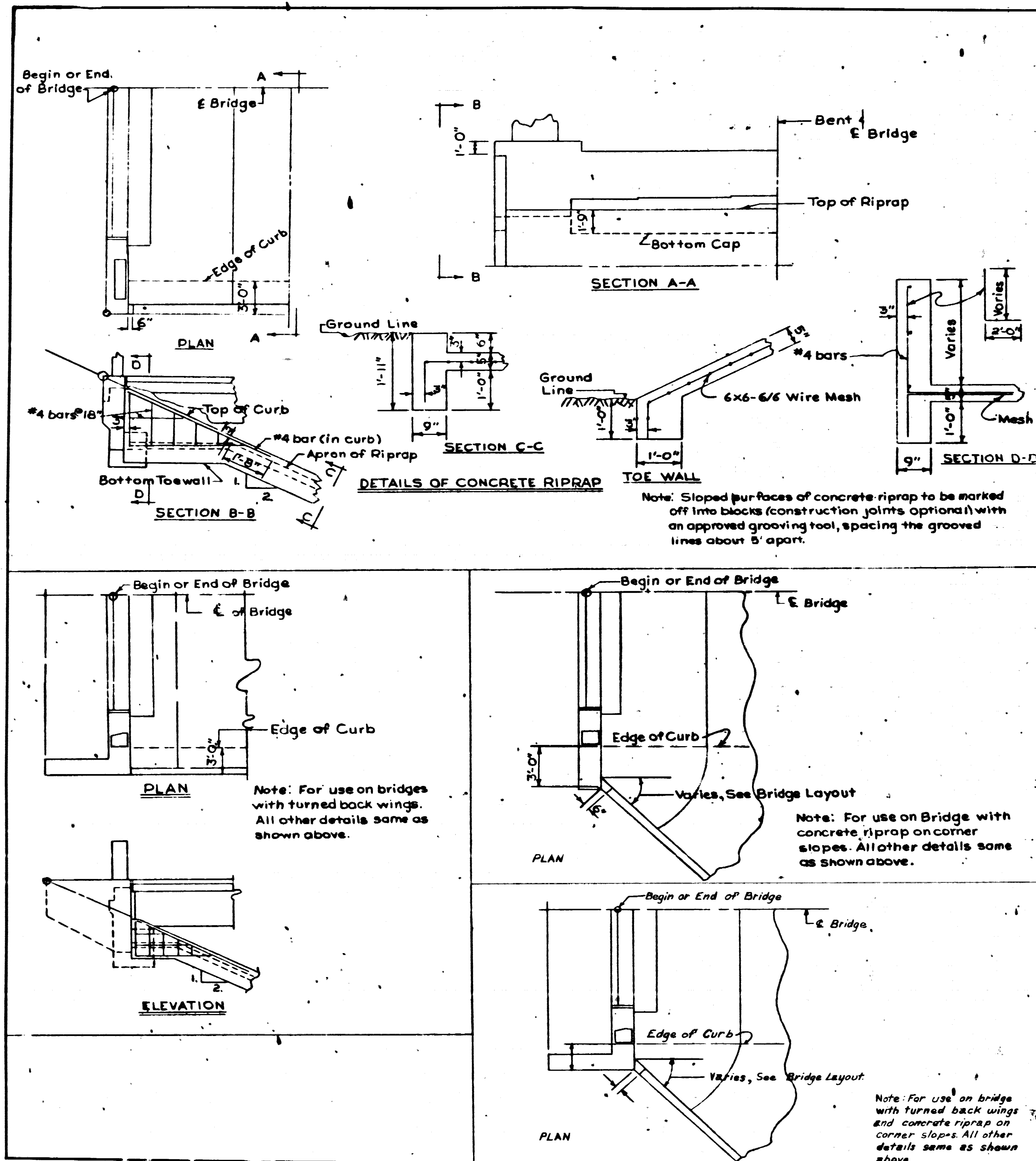
MATERIALS FOR TUGGLE BOLT: TUGGLE BOLT ASSEMBLIES SHALL BE MANUFACTURED OF GALVANIZED OR STAINLESS STEEL, OR OF ALUMINUM. THE ENTIRE ASSEMBLY SHALL BE CAPABLE OF WITHSTANDING A TENSILE LOAD OF 9000 POUNDS APPLIED THROUGH A 1" Ø HOLE. THE MATERIALS FOR COMPONENTS WILL BE SPECIFIED BY RECOGNIZED ASTM DESIGNATIONS ON SHOP DRAWINGS SUBMITTED FOR APPROVAL, AND WILL BE ACCEPTED BY MANUFACTURER'S CERTIFICATION ON APPROVAL BY THE ENGINEER.

1. 9-15-78 Revised for 1978 Specs KDH

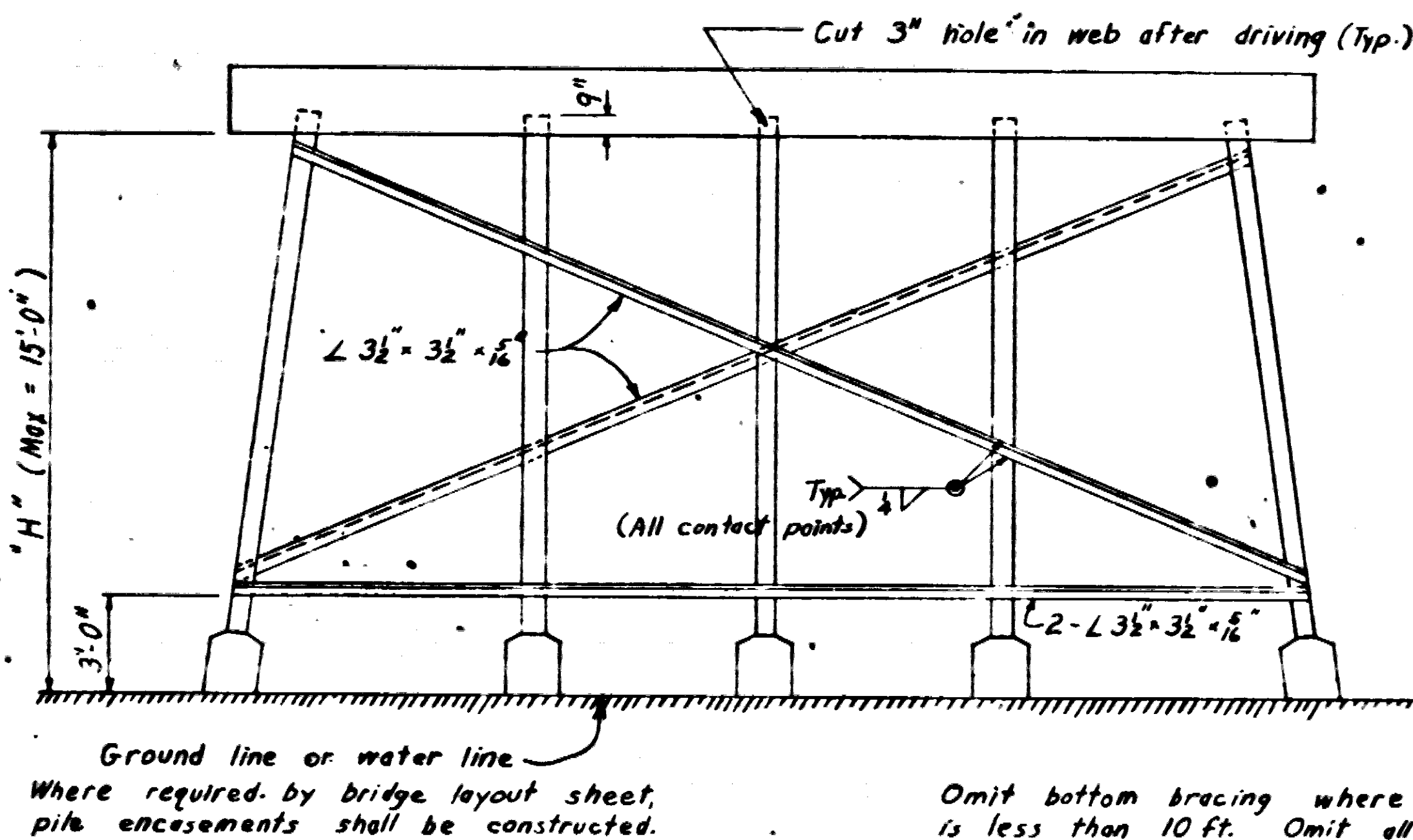
DETAILS OF  
METAL BRIDGE RAILING  
TYPE "B" "C" "D" "E"  
ROUTE SEC.  
ARKANSAS STATE HIGHWAY COMMISSION  
LITTLE ROCK, ARK.

DRAWN BY: W.W.W. DATE: 9-25-70  
 TRACED BY: \_\_\_\_\_ DATE: \_\_\_\_\_ SCALE 1/2"=1'-0" or as noted  
 CHECKED BY: DPL DATE: 10-5-70  
 BRIDGE NO. \_\_\_\_\_ DRAWING NO. 149935



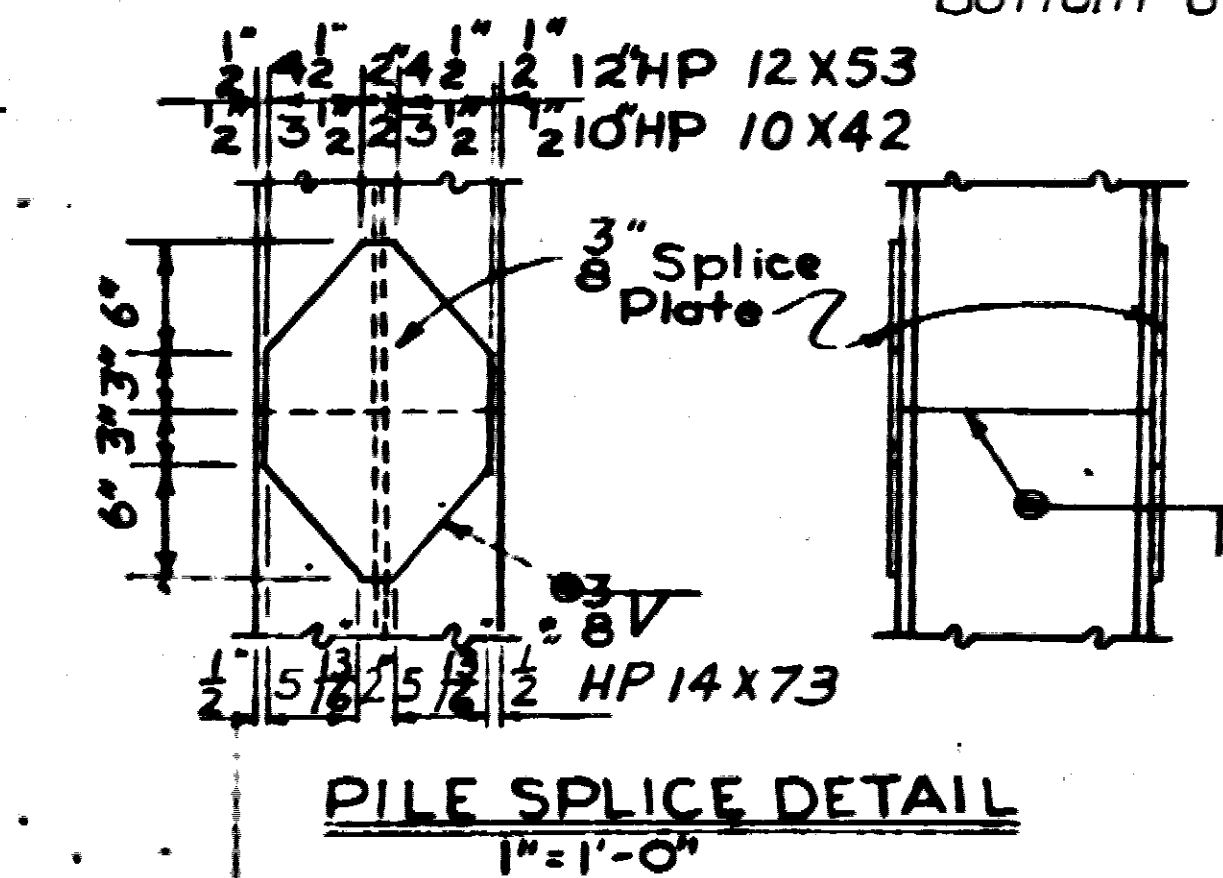


All bracing shall be cut and welded in the field. Each brace shall be furnished in one piece. Payment shall be made under item 807.



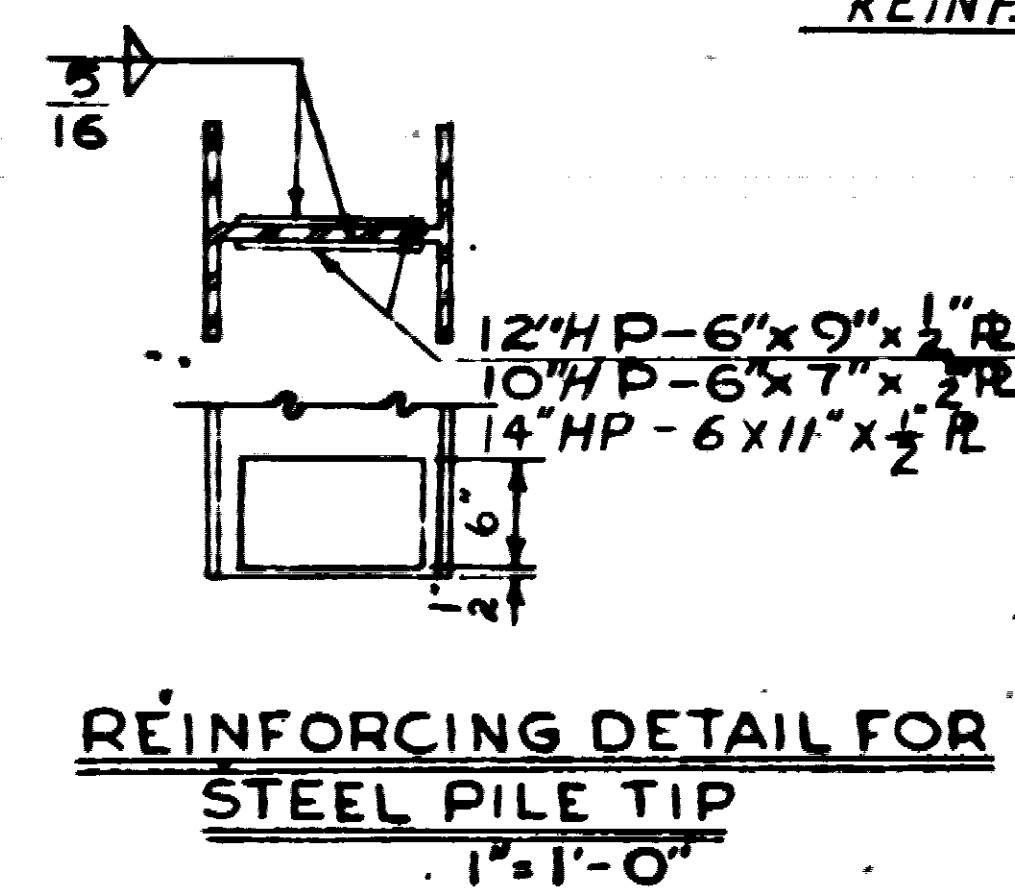
#### TYPICAL BRACING FOR INT. STEEL PILE BENTS

Note: Omit Bracing where Pile Encasement is Extended to Bottom of Bent Cap.



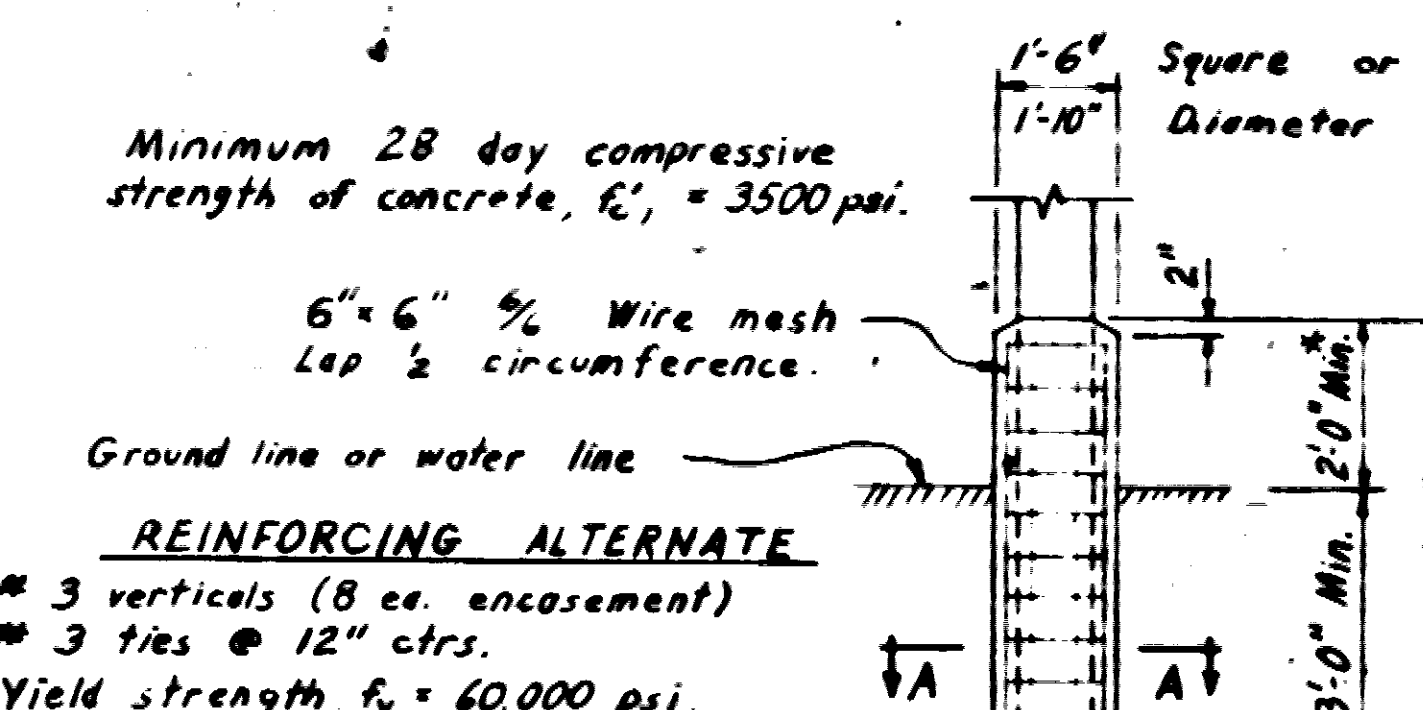
The Contractor may for his convenience and at his own expense provide as many as three splices per pile for steel bearing piling. Minimum spacing between splices shall be 5 feet.

NOTE: Drawing Adapted From Drawing No. 14995, With Detail Drawing Concrete Riprap on Corner Slopes L.E.G. 2/16/72



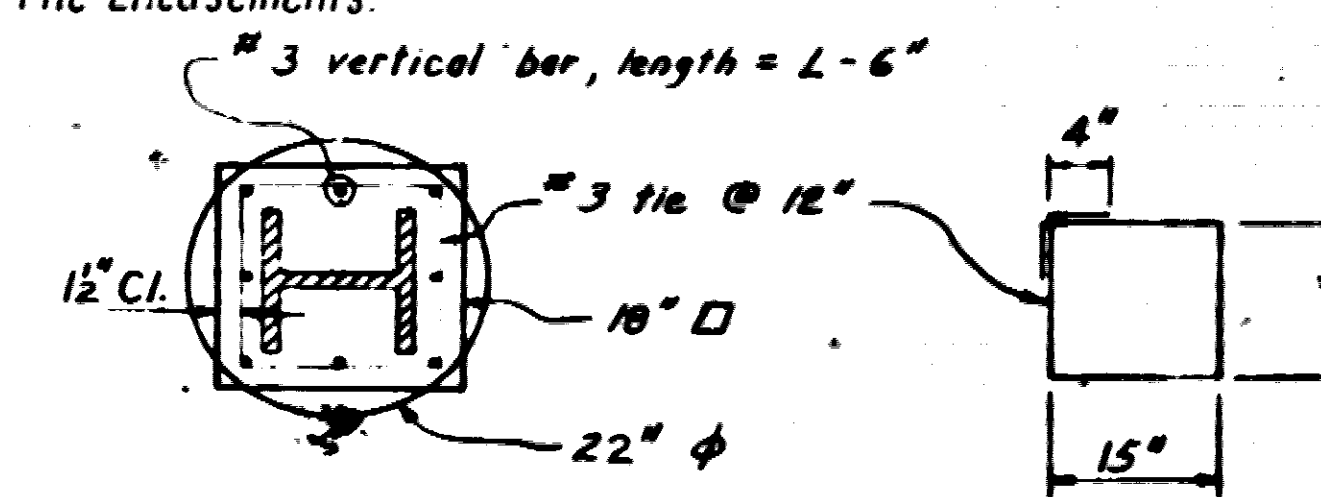
Note: Steel Pile Tip Reinforcing will not be paid for directly, but shall be considered subsidiary to the item of "Steel Bearing Piling".

DATE	REVISED	DATE	REVISED	DATE	REVISED	DATE	REVISED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
2/15/72		10/5/76		12-5-77		500-10-577		6	ARK.			34	
4-21-76		10/5-21-76		5-7-81		10/1-5-141							
9-24-77		10/9-10-77		2-22-82		507-2-22-82							



#### PILE ENCASEMENT DETAIL

NOTE: Concrete and Wire Mesh or Reinforcing in Encasements shall be paid for at the contract unit price per linear foot bid for "Pile Encasements".



#### DETAILS OF CONCRETE RIPRAP AND MISC. DETAILS OF STEEL PILING ROUTE SEC.

ARKANSAS STATE HIGHWAY COMMISSION  
LITTLE ROCK, ARK.

DRAWN BY: J.L. DATE: 6-6-69  
TRACED BY: DATE: 6-6-69  
CHECKED: EMH DATE: 6-24-68  
BRIDGE NO. SCALE: NO SCALE  
DRAWING NO. 14995A